



Bellevue Planning Commission

Wednesday, January 13, 2016

6:30 to 8:45 p.m. ■ 1E-113

City Hall ■ 450 110th Avenue NE, Bellevue

Agenda

Regular Meeting

- | | |
|-----------|---|
| 6:30 p.m. | 1. Call to Order
<i>Michelle Hilhorst, Chairperson</i> |
| | 2. Roll Call
<i>Michelle Hilhorst, Chairperson</i> |
| | 3. Approval of Agenda |
| 6:35 p.m. | 4. Public Comment*
<i>Limited to 5 minutes per person or 3 minutes if a public hearing has been held on your topic</i> |
| | 5. Communications from City Council, Community Council, Boards and Commissions |
| | 6. Staff Reports
Quarterly Check-in on Progress, Procedures & Support <ul style="list-style-type: none">• Topics covered in packet include: Accomplishments from last quarter, Commissioner work on discrete tasks, public comment, speaker series, off-site meetings, use of data, and visual tools |
| | 7. Draft Minutes Review
December 9, 2015 |
| | 8. Study Session |
| 7:15 p.m. | A. Downtown Livability
Review of how code elements (standards, guidelines and incentives) fit together to make a project and their relationship to livability; how district identity and street character are proposed to be addressed in the code update
<i>Patti Wilma, Community Development Manager</i>
<i>Emil King, Strategic Planning Manager</i>
<i>Patricia Byers, Code Development Manager</i> |
| | 9. Public Comment* - Limited to 3 minutes per person |
| 8:45 p.m. | 10. Adjourn |

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Agenda times are approximate

Planning Commission members

Michelle Hilhorst, Chair
John deVadoss, Vice Chair
Jeremy Barksdale
John Carlson

Aaron Laing
Anne Morisseau
Stephanie Walter

John Stokes, Council Liaison

Staff contacts

Emil King, Strategic Planning Manager 425-452-7223
Michael Kattermann, Acting Comprehensive Planning Manager 425-452-2042
Michelle Luce, Administrative Assistant 425-452-6931

** Unless there is a Public Hearing scheduled, "Public Comment" is the only opportunity for public participation.*

Wheelchair accessible. American Sign Language (ASL) interpretation available upon request. Please call at least 48 hours in advance: 425-452-5262 (TDD) or 425-452-4162 (Voice). Assistance for the hearing impaired: dial 711 (TR).



January 13, 2016

SUBJECT

Downtown Livability Initiative

STAFF CONTACT

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Development Services

DIRECTION NEEDED FROM PLANNING COMMISSION

- Action
- Discussion
- Information

Tonight's Commission study session will be divided into two sections:

- 1) Key contextual information that will help guide the Commission's work on Code development over the coming months; and
- 2) Desired early direction from the Commission on Neighborhood Character and Streetscape Standards.

BACKGROUND

In December 2015, the Commission formed a set of recommendations of "early win" code amendments. Staff is in the process of drafting the "early wins" transmittal package to Council. The "early wins" included subjects such as the permitted use charts, mechanical equipment screening and location, signage for publicly accessible spaces, street trees and planter strips, extension of the pedestrian corridor to 112th Avenue NE, weather protection, and aligning the Downtown boundary with the Comprehensive Plan Update from last summer. The Planning Commission held three study sessions and a public hearing to discuss these subjects before forming their recommendations. The City Council is anticipated to take action on the Commission's "early wins" recommendations in the first quarter of 2016.

The Planning Commission is now shifting to the larger, more complex topics that were part of the Downtown Livability Citizen Advisory Committee (CAC) recommendations. Staff anticipates the Commission's work to take a number of months and involve significant review,

analysis and public engagement. The Commission will ultimately form a recommended Code and design guideline package to transmit to Council for final action.

DISCUSSION

The major Downtown Livability Initiative topics that are before the Planning Commission to consider in the coming months are design guidelines, open space, height and form, and the amenity incentive system. In order to consider these topics, it is helpful to have a clear understanding of how Bellevue's Land Use Code framework is used to review project development proposals. It is also important to keep in mind that the Land Use Code and updates to development codes in general are just a part of the broader livability effort for Downtown. See **Attachment A** for information on broader efforts. This information will build on the earlier material that was presented to the Commission last summer.

1A. CONTEXTUAL INFORMATION

Process Overview

The pre-application meeting is the first official meeting for large projects between project applicants and the Development Services' review staff. All development review groups are represented (Land Use, Transportation, Building, Fire, Clear and Grade, and Utilities Review). Each of the reviewers looks at the viability of the project and provides written comments to the applicant regarding the project. Major site and access issues are reviewed, along with bulk and height. Specific design details are not covered at this stage of the review. The City offers this service free of charge and provides a consolidated response from all of the reviewers to aid the applicants as they prepare their project documents for Design Review submittal.

The next step is application submittal. All new Downtown projects go through design review and SEPA (State Environmental Policy Act) threshold determination review concurrently. When the project is submitted for Design Review it is reviewed by the same review groups as noted above. The Land Use and Transportation review groups conduct an in-depth review of the project to ensure that the project complies with Bellevue's Land Use and Transportation Codes. The rest of the review groups also review the submittal, but are looking at it from a high level and reviewing it for problems that would stop the project from going forward. The most in depth review for the other departments is done during the construction permits stage of the project.

Land Use planners are the project managers throughout the Design Review stage and coordinate the review and revision cycles of all the other reviewers. After review is complete, the Land Use planner will issue the Director's Decision in a written staff report along with the SEPA Threshold Determination. The staff report contains an analysis of the project and its compliance with code, as well as any associated conditions of approval.

Within the Director's Decision is the SEPA analysis. SEPA covers all of the environmental issues that are not covered by other environmental laws such as critical areas. For Downtown projects, the most common environmental impacts encountered in SEPA review are traffic and noise. After evaluating the proposal and identifying mitigation measures, the lead agency (City

of Bellevue) must determine whether the project would still have any probable significant adverse environmental impacts. The City of Bellevue issues either a determination of non-significance (DNS) or if the proposal is determined to have a probable significant adverse environmental impact then a determination of significance/scoping notice (DS/Scoping) is issued and the environmental impact statement (EIS) process is begun. In most Downtown development, a DNS is issued.

Role of the Comprehensive Plan & Downtown Subarea Plan

During the review of development projects, land use planners and other reviewers must determine whether the project is in compliance with Bellevue's Comprehensive Plan goals and policies that direct the City as to how development will occur in the future. They provide specific guidance for development regulations and administrative decision making. The Comprehensive Plan is comprised of the general elements (Land Use, Transportation, etc.) as well as subarea plans, including the Downtown Subarea Plan. The subarea plans are similar to the general Comprehensive Plan elements in that they set forth general community goals and policy, but they apply to a specific area such as Downtown.

Relationship of Development Standards, Incentives, and Design Guidelines

A key part of the Commission's upcoming work is developing updated development standards, guidelines and incentives, with consideration of their interplay how they work together to support a livable Downtown. Staff will review these and their inter-relationships with the Commission on January 13.

A. Development Standards

Development standards are requirements for all development unless there is an exception. These include requirements such as allowed uses setbacks, lot coverage, landscaping, and parking. When a project is reviewed, the land use planner ensures that the project complies with all of these standards.

In Downtown Bellevue, there are Downtown land use districts, or zoning districts, based on the Downtown Subarea Plan and implemented by the Land Use Code (LUC). These include the O-1, O-2, MU, OLB, OB and R districts along with perimeter district overlays.

Dimensional standards for height, floorplates, setbacks, lot coverage, and floor area ratio (FAR) work together to define a building form. There are maximum allowable floorplates for residential and nonresidential buildings, floorplate sizes reduce in area above 40 feet and reduce again above 80 feet in height to reduce the bulk of taller buildings. In addition to the reduction in floor plate sizes, there are multiple conditions where the building has mandated setbacks from the property line at a certain height to promote light and air at the sidewalk and between buildings. Exceptions or flexibility also provided such as allowed increase in height, floor plate, or FAR depending on design criteria or performance standards.

Density in Downtown Bellevue is expressed by Floor Area Ratio (FAR). It is computed by gross floor area in square feet excluding parking and mechanical areas, divided by the site

area in square feet. The LUC provides basic and maximum allowable FAR for both residential and nonresidential buildings.

B. Incentives

As has been discussed, the amenity incentive system provides a way for applicants to earn extra FAR up to the maximum FAR allowed. There are currently 23 amenities that are a part of the system, with a number of new ideas generated by the CAC.

C. Design Guidelines

The design guidelines help to implement the community vision for the area. They are flexible and do not prescribe specific design solutions. Rather, the guidelines are a descriptive template for promoting and improving the urban character of the area without dictating or prescribing a specific style or theme. The design guidelines for downtown Bellevue include Building/Sidewalk Relationships, Perimeter Design District, Core, Old Bellevue, Civic Center, and the Pedestrian Corridor Guidelines and Major Public Open Spaces.

1B. THE PEDESTRIAN REALM AND URBAN FORM

This next section of the memo shifts to urban design inter-relationships. A partnership of development standards, incentives, and design guidelines are needed to create a livable, vibrant, and attractive Downtown. **Attachment B** provides an in-depth look at the key elements of the pedestrian realm, streetscape, and at key elements of what goes into a creating a livable and vibrant Downtown. The Planning Commission is asked to review this information in preparation for the February meeting which will focus on recommended standards and guideline changes to the Land Use Code that are intended to improve the livability of Downtown.

2A. NEIGHBORHOOD CHARACTER

Staff is asking for early Commission input on Neighborhood Character to prepare for development of Code amendment language.

Neighborhood Character CAC Recommendations

Neighborhood character is called out in many of the topic areas covered by the CAC.

- **Open Space Strategy 1:** Identify and incentivize different open space expressions for each neighborhood to help each neighborhood's needs and enhance character. (pg. 14 and 15 CAC Final Report)
 - This will be addressed through the FAR amenity incentive system work later this winter.
- **Open Space Strategy 2:** Strengthen requirements and guidelines for integrating through-block connections internal to superblocks...reinforce district identity. (pg. 16 CAC Final Report)
- **Design Guideline Strategy 2:** Refine content of design guidelines... to reinforce neighborhood character. (pg. 30 CAC Final Report)

- **Amenity Incentive System Strategy 3:** Consider neighborhood-specific weighting of amenities. (pg. 39 CAC Final Report)
- **Height and Form Analysis Area 2D:** Perimeter Design Districts on the edges of Downtown; no change to the “B” except for the arced called “Deep B” and a possible height increase to 70 feet in the A District. City Council said no change to height should be made if it did not also enhance the adjacent residential neighborhoods.

Downtown Neighborhoods were initially identified in the Downtown Subarea Plan as a key piece of the Great Place Strategy. The distinct neighborhoods capitalize on their locations and unique identities and break down the 600-foot superblock pattern into pedestrian-friendly size districts. Some of the neighborhoods are already well defined such as Old Bellevue and Ashwood. Others will develop over time. Therefore establishing the character and intent of neighborhoods is important to setting context so Design Guidelines for infrastructure, development, open space, amenities, uses, and activities can be tailored to them.

See **Attachment C** for the Neighborhood Character Intent Statements proposed to be included in the Land Use Code and be a foundation for open space, design guideline, amenity and height and form work to be done later this year.

Recommendations by staff are included and are based on current Comprehensive Plan language, such as reference to the Lake to Lake Trail, CAC and stakeholder discussion about urban form, and reference to light rail access.

2B. STREETSCAPE STANDARDS AND GUIDELINES

Staff is asking for early Commission input on Streetscape Standards and Guidelines to prepare for development of Code amendment language. Streetscape standards and guidelines apply to right-of-way designations that provide a hierarchy of street type having to do with intensity of pedestrian activation and friendliness, breaking down the superblock with through-block connections, and sidewalk widths to accommodate comfortable movement Downtown. The initial step in refining guidelines is to establish fundamental standards to which guidelines will then apply. Proposed refinements to guidelines based on what is presented in this memorandum will be presented later this winter. This topic also incorporates recommendations from the Transportation Commission’s Downtown Transportation Plan Update.

CAC Recommendations

The following CAC recommendations relate to the streetscape and pedestrian environment.

1) Building/Sidewalk Right of Way Designations

Design Guidelines Strategy 2: Refine content of design guidelines.

- a. Update building/sidewalk right-of-way relationships (pg. 30 CAC Final Report) Better depict where highest level of pedestrian activity area to be concentrated. Ensure building frontages are pedestrian friendly. Requiring weather protection was included in “early wins”. Clarify expectations and provide flexibility.

See **Attachments D-1 and D-2** for proposed right-of-way designations and standards.

A few recommendations by staff are included and are from follow-through on the CAC Final Report. They are intended to provide clearer direction on location of parking and vehicular access and ground floor frontage heights.

2) **Through-block Connections**

Open Space Strategy 2: Strengthen requirements and guidelines for integrating through-block connections internal to superblocks. (pg. 16 CAC Final Report)

Standards to focus on smaller block pattern for pedestrians, activate edges, connect open spaces, maximize sunlight, and accommodate bicycles, pedestrians, and motor vehicles.

Design Guidelines Strategy 2: Refine content of design guidelines. (pg. 30 CAC Final Report)

- a. Pedestrian circulation / through-block connections.

Map through-block connections. Ensure they are safe and friendly. Create options for design to help integrate into development allowing flexibility for unique site conditions.

Provide common wayfinding. Use ADA and CPTED principles. Recommend dimensions as shown in the CAC Final Report.

See **Attachment E-1 and E-2** for proposed through-block connections map and standards. The **Attachment E-2** map is the same as that on pg. 16 of the CAC Final Report except all through-block connections are shown at dotted lines to differentiate them from streets.

3) **Sidewalk widths: Integration with Downtown Transportation Plan (DTP)**

(Text from the DTP Recommendation)

Sidewalks in Downtown Bellevue provide the fundamental infrastructure for pedestrian mobility and incorporate urban design features that enhance livability. The Downtown Land Use Code prescribes the width of sidewalks and the landscaping treatment adjacent to the street. Both the private sector and public sector must incorporate the Code provisions in new buildings and infrastructure projects.

The Transportation Commission forwarded a recommendation to the Downtown Livability Initiative Steering Committee to amend the Land Use Code to increase the required sidewalk width along certain heavily travelled street segments such as along 106th Avenue NE where 12-16 foot wide sidewalks would accommodate lots of pedestrians, window shoppers and café seating on this designated “Entertainment” Avenue. Along some streets where there is no on-street parking and where a buffer is needed from traffic, the Commission recommended substituting a continuous landscape planter along the outside edge of the sidewalk instead of street trees in tree grates. This type of treatment is popular with pedestrians along portions of Bellevue Way and NE 4th Street.

Increasing landscaping along sidewalks to reduce impervious surface and buffer pedestrians from traffic was included in “early wins”.

See **Attachment F-1** proposed complete sidewalk width changes and **Attachment F-2** for only the sidewalks proposed to be increased in width.

Additional sidewalk width would come from behind the curb line. Work on base FAR and other development standards will address the effect this may have on development potential.

Recent development has provided sidewalk widths as currently required by the Land Use Code and they may stay as-is. Increased sidewalk width would apply to only new development.

Recommendations by staff related to new Comprehensive Plan language and access routes to future light rail station are included and are highlighted in **Attachment F-2**.

NEXT STEPS

In the upcoming months staff plans to bring forward materials in this order:

- The Public Realm – further detail on the relationship of the pedestrian environment to the buildings and open space around it
- Height and Form – further detail and analysis of the potential additional height and FAR, tower form, and mitigation of impacts
- Open Space - public, private, active, passive, location, type, priority in specific neighborhoods
- Incentive System – nexus and proportionality, relationship to height and FAR, amenities, public benefit, economic analysis, type and location
- Final DRAFT code language

Standards and guidelines will be presented for discussion at the same time for each of these topics as they go hand in hand to ensure and enhance livability.

Update on Broader Livability Efforts for Downtown Bellevue

May 2015

The current work on updating the Downtown Land Use Code through the Downtown Livability Initiative is part of a broader agenda to make Downtown more people-friendly, vibrant and memorable, and add to the amenities that make for a great city center. This includes a wide variety of new and upcoming projects, programs and events in the following categories that provide on-the-ground examples of how the City and other participants are furthering livability in Downtown Bellevue beyond the Code update.



Safety and security

Council has set aside funding to begin site selection for a **new Fire Station #10** to serve Downtown and the surrounding area. This

funding will secure land for a facility to provide rapid response for fire and emergency medical calls. The Police Department has a designated **Downtown Policing Squad** comprised of a lieutenant, corporal, and four officers.



Schools

Over 1,000 children (ages 0-18) are currently living Downtown per City estimate. The City has worked with the Bellevue School District and they

recently identified a site at 124th Ave NE and Main Street for a **new elementary school** to serve the growing number of Downtown families. Anticipating this need, the school was fully funded in the district's 2014 capital construction bond.



Walkability/ pedestrian comfort

Creating a safer, more convenient and inviting pedestrian environment is a prime goal of the City. Current

projects to improve walkability and implement the Downtown Transportation Plan include: **enhancing the pedestrian crossing at 108th Ave NE/NE 4th Street**; **improving the intersections for pedestrians on the south side of Downtown Park**; **elevating the 106th Ave NE/NE 6th Street intersection** adjacent to Compass Plaza to create a safer raised "table"; and a wider and **gently sloping ramp in the "Garden Hillclimb"** segment of the Pedestrian Corridor. The City is also **following up with improvements for people that are mobility-impaired**. Council provided a dedicated resource in the new Capital Investment Plan adopted in December 2014 for addressing citizen accessibility requests and implementing high priority improvements in the City's ADA Transition Plan.



Character

One of Council's adopted priorities for 2015-16 referred to as the **Grand Connection** will create major placemaking elements and open space

from Meydenbauer Bay, through Downtown, and across I-405 to the Eastside Rail Corridor trail (old Burlington Northern rail line). Conceptual design work will be proceeding in 2015 on what will become a signature feature for Downtown. The **Old Bellevue identity project** including gateway signage, district markers, and historical utility box wraps was completed in early 2015. A downtown-wide **median study** will prioritize planted median locations that will green the Downtown, aid in pedestrian safety and enhance neighborhood and gateway character. Implementation will take place in coordination with existing and planned transportation improvements.

Update on Broader Livability Efforts for Downtown Bellevue *(continued)*



Public transit

Council and the Sound Transit Board recently approved an updated Memorandum of Understanding for the **East Link**

light rail project. Light rail is now moving forward with two stations to serve Downtown, at East Main Street and NE 6th Street. In 2023, the system will open with a dedicated guideway to downtown Seattle, Redmond's Overlake District, and the rest of the regional high capacity transit system. Work has been advancing on bus transit as well. The Downtown Transportation Plan identified access improvements and transit route modifications that will provide 97 percent of Downtown residents and employees with a short walk to a transit stop or station. The 108th Ave NE corridor study mentioned above will address transit speed and reliability issues. Work on pedestrian and bicycle facilities at the **Bellevue Transit Center** will help implement the Council direction to provide exceptional access to the Downtown light rail stations.



Vehicular mobility

A number of street projects are underway or in the works to **improve vehicle access to**

Downtown. The new NE 4th Street extension from 116th Ave NE to 120th Ave NE, and the widening of 120th Ave NE, add capacity for traffic entering and exiting Downtown. Both will be completed in 2015. Full funding is now in place for the Spring Boulevard connection between NE 12th Street and 120th Ave NE, which will provide additional capacity at the north end of Downtown. Council also recently approved funding for full design and environmental review of **high-occupancy vehicle (HOV) improvements on Bellevue Way just south of Downtown**, which would add significant capacity to move people during the evening commute period. In the Old Bellevue area, **25 new on-street parking spaces** have been added to support local small businesses, as recommended in the companion Downtown Transportation Plan.



Bicycle mobility

The Downtown Transportation Plan provides for **enhanced bicycle circulation** both within Downtown and connecting to surrounding

neighborhoods and to regional bicycle facilities along SR 520 and I-90. Projects at 108th Ave/Main Street and NE 8th Street/112th Ave NE will improve safety for people riding bicycles. The City recently installed bicycle wayfinding on 108th Ave NE between Main Street and I-90, and is working with Redmond on **bicycle wayfinding between the downtowns of the two cities. A corridor planning effort is about to get underway that will address the multimodal mobility needs for 106th Ave NE, 108th Ave NE and Main Street.**



Parks and open space

Council approved funding and work is expected to be underway this summer to **complete Downtown**

Park, which includes constructing the remaining portion of the circular water feature and promenade, and completing a "gateway" at the Park's northeast quarter fronting on Bellevue Way. The Rotary Club of Bellevue has partnered with the City to help fund and build **Inspiration Playground** within Downtown Park. It will feature universally accessible design and sensory-rich play experiences suitable for all ages and abilities. Work also continues on the **first phase of Meydenbauer Bay Park**, which will result in a spectacular connection between Downtown and the Lake Washington waterfront.



Cultural facilities

KidsQuest Children's Museum is planning to open their new facility Downtown in the Ashwood Neighborhood in the first half of

2016. A new community-based 3,200 square foot **performing arts venue named Resonance** recently opened in the SOMA Towers, through the efforts of Su Development. **Meydenbauer Convention Center** is scheduled to begin a \$12.5 million renovation in 2015 that will enhance the facility's atmosphere and deliver the latest technology upgrades to support events. **Council recently offered to provide up to \$20 million towards construction of the Tateuchi Center**, a proposed 2,000-seat regional performing arts center. The City's offer is contingent on other non-City funds being raised by September 2016, having a City ownership interest in the facility, and a commitment for 50 years of operation and public programming.



Affordable/ workforce housing

Downtown is Bellevue's fastest growing neighborhood and the location planned for the largest share

of the City's residential growth. As part of a broader City housing strategy, Bellevue is considering use of a **multifamily tax exemption (MFTE) program** to encourage development of workforce housing in Downtown, Bel-Red, Eastgate, Newport Hills Commercial Area, Crossroads, and Wilburton Commercial. The Low Income Housing Institute (LIHI) recently completed **August Wilson Place**, which adds 57 units of affordable workforce housing to Downtown, including 12 units for homeless people, eight units for veterans, and three units for families with developmental disabilities.



Entertainment/events

A variety of entities such as the Bellevue Downtown Association, Old Bellevue Merchants

Association, private groups and sponsors, and the City of Bellevue provide or a wide range of events and entertainment in Downtown. These include the **Bellevue Farmers Market, Bellevue Jazz and Blues Festival, Live at Lunch, Four on the 4th Dog Jog & Walk, ChowDown(town) Food Truck Round-up, and Old Bellevue's Taste of Main.**



Neighborhood services

To serve the needs of residents living Downtown and the surrounding neighborhoods, a third major grocery store opened in the subarea. **The**

Asian supermarket Hmart now joins QFC and Safeway in Downtown Bellevue. In addition, Downtown continues to **evolve with an ever-growing mix of retail, restaurants and coffee shops, and entertainment venues** that offer services to Downtown residents, workers, and nearby neighborhoods as well as the rest of Bellevue and the region.

1 - PEDESTRIAN REALM

ELEMENTS OF URBAN FORM

Why it is Important

Many design decisions help contribute to a comfortable and engaging pedestrian realm. First and foremost, the pedestrian realm should be clearly defined within the streetscape to maintain safety. Within the pedestrian realm, design decisions should maintain clear zones to allow for easy pedestrian mobility, spaces for pedestrians to rest, as well as provide regular points of interest. The pedestrian realm along streetscapes not only includes horizontal areas from street curbs to building facades but also includes vertical areas defined by street tree canopies, weather protection, and the first to second stories of adjacent buildings. Incorporating the design of both vertical and horizontal elements within the pedestrian realm helps define and enrich the pedestrian experience and promote pedestrian activity.

Bellevue, as a “City in a Park,” values open space as a key component to the character of Downtown. With long superblocks throughout much of Downtown, features such as plazas, through-block connections, and midblock crossings are important elements in the pedestrian realm. They offer easy connections across long blocks and opportunities to move through the center of a block away from busy traffic. In some locations, these areas are shared space used by vehicles, bicyclists, and pedestrians and in others, they are pedestrian only areas. Where multiple uses occur, pedestrians, bicyclists and motorists need to clearly understand that these locations are shared spaces.

Design of the Pedestrian Realm

The pedestrian realm is made up of many elements. It includes sidewalks, plazas, crosswalks, midblock crossings, through blocks, parks, and other areas open for public access. A network of sidewalks forms the main routes of connection throughout the Downtown, with through-block connections, and other public open space providing opportunities for pedestrians to separate from street traffic, and in many cases, shorten walking distances. The high quality design of all of these areas helps improve the livability, memorability and viability of Downtown.

Building Sidewalk Relationship Guidelines

To help create an engaging pedestrian realm, there needs to be a relationship between the level of pedestrian use, visual and physical access, and weather protection. The relationship is simple: the more pedestrian use along a particular building frontage, the more weather protection and visual and physical access. This relationship is called the Building Sidewalk Relationship. The CAC reviewed designations (pg. 32 of the CAC final

report) throughout the Downtown and made recommendations regarding the minimum % of weather protection or visual and physical access that a building facade is required to have.

Sidewalk Zones

Sidewalks are typically composed of several zones that serve a critical function in supporting a vibrant pedestrian experience. These zones include the Frontage Zone, Through Zone, and the Buffer Zone (See Figure 1.1 for Sidewalk Zones and Dimensions).

The Frontage Zone is the zone closest to a building that is intended to allow for window shopping, areas for people to stand, café seating, among other functions. The building façade, a key component to the pedestrian realm, is considered as a part of this zone, as it often defines the edge of the pedestrian realm and provides opportunities for interesting points of interest and interaction for those on the sidewalk. The size of this zone will vary depending on the dimensions of the street.

The Through Zone is the primary path of pedestrians as they move down along a street. This zone should be clear of obstructions to allow for such movement. The National Association of City Transportation Officials (NACTO) suggests that this zone should be a minimum of eight feet in downtown or commercial areas. In many locations, current required sidewalk widths within Downtown Bellevue do not satisfy this minimum dimension without completely eliminating or impinging upon other important zones within the sidewalk. Given other minimum dimensions that are required, such as those laid out by the Americans with Disabilities Act (five feet in width required to turn around a wheelchair), it is feasible to have a smaller Through Zone, but these areas can become congested with high pedestrian use.

The Buffer Zone is typically located immediately adjacent to the street separating the Through Zone from traffic lanes. This zone is intended to buffer pedestrians from nearby passing automobiles. This zone includes permanent elements like places to lock up bicycles, public art, lighting, utility poles, street furniture, tree pits, above-ground planters, and landscape strips. It is important to note that elements in this zone, such as street furniture, above-ground planters, and public art, can be temporary or movable. Such features add interest to the streetscape and help activate the pedestrian realm. Trees located in tree pits and landscape strips typically dictate the size of the Buffer Zone. If tree pits are undersized they can damage sidewalks and create tripping hazards. Such conflicts can become a safety concern as street trees tend to lift sidewalk panels if they are not given sufficient room to grow.

CAC References

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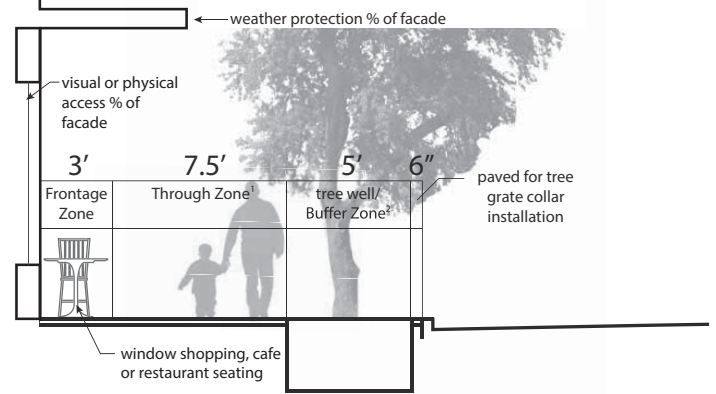
Land Use Code Reference

20.25A.060

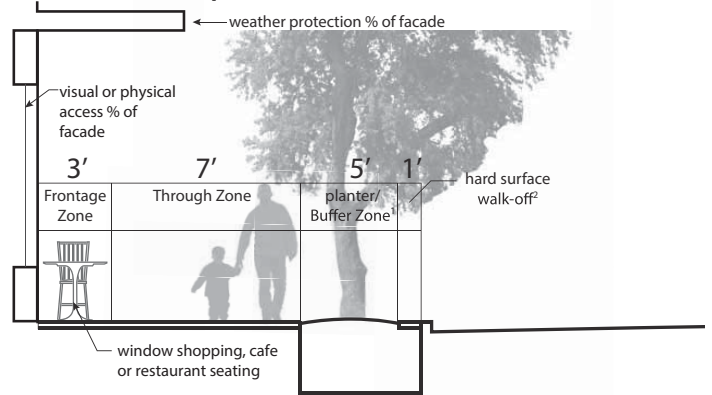
Figure 1.1 - Sidewalk Zones and Dimensions

The diagram below shows dimensions of each sidewalk zone when applied to 12', 16' and 20' sidewalks. It is important to not that on all streets designated for 12 foot sidewalks, cafe and restaurant seating would reduce the Through Zone below acceptable minimums and could conflict with ADA requirements.

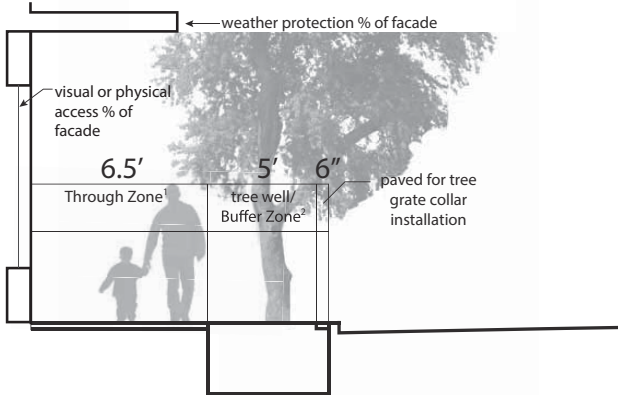
16' sidewalk with tree wells



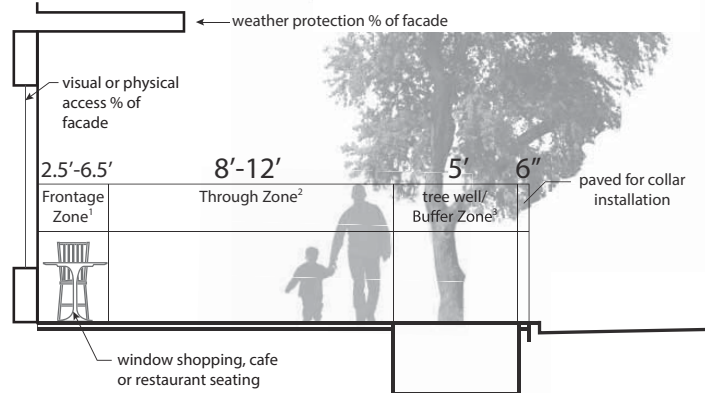
16' sidewalk with onstreet parking + continuous planter



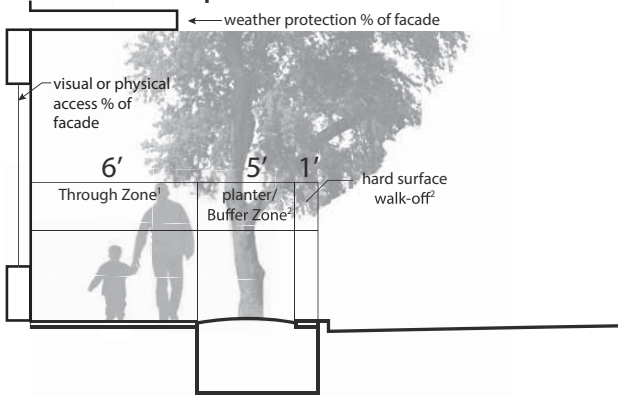
12' sidewalk with tree wells



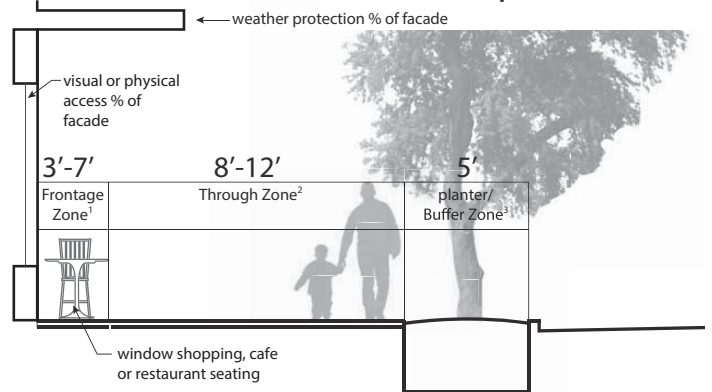
20' sidewalk with tree wells



12' sidewalk with onstreet parking + continuous planter



20' sidewalk with continuous planter



¹ = NACTO guidelines state that a minimum Through Zone dimension should be eight feet in downtown areas. In calculating the Through Zone dimensions, it is possible to include up to one foot of a five foot wide tree grate as a part of the Through Zone.

² = Buffer Zones should include utilities, bicycle racks, street furniture, public art and other amenities.

Why it is Important

To preserve and enhance the quality of life for those who live, work, and visit Downtown, providing opportunities for access to sunlight, sky views, and privacy are essential. Tower spacing plays a critical role in preserving and enhancing these elements, in addition to the scale of the pedestrian environment. Towers with inadequate separation can create adverse impacts on the public realm through excessive shade and shadow, obstruction of adequate sky views, and a scale that is detrimental to a pleasurable pedestrian environment. Appropriate tower separation can improve these conditions while also enhancing the quality of the interior environment by providing improved access to daylighting and privacy. Bellevue does not have a precise requirement for tower separation, rather relying on stepback requirements and the International Building Code to establish a minimum 40' separation. Figure 2.1 illustrates comparisons between International Building Code requirements and best practices found in other cities.

The primary objectives of providing appropriate tower separation are:

Sunlight

A rich network of public spaces interconnects the fabric of Downtown, working in support of streetscapes and other public open spaces. Sunlight is an essential element to activating the public realm. When towers are spaced too closely opportunities for sunlight to penetrate to the ground level is significantly diminished.

Scale

When separation is not adequately provided an overwhelming and constrained pedestrian environment can be established. Public spaces such as plazas, parks, through block connections, and streetscapes can appear uninviting, unsafe, and uncomfortable. Appropriate tower separation can establish relief from the overall massing while emphasizing a pedestrian scaled podium.

Privacy

An issue primarily relative to residential developments, appropriate tower spacing can be an integral element to establishing privacy. Appropriate orientation, placement, and spacing can enhance a sense of privacy between residential and office buildings.

Sky Views

Visual access to the sky is important for not only sunlight, but enhancing the feeling of openness and connection to environmental conditions such as weather and sunlight. In a dense urban environment, the clustering of high rise buildings can often create a tight sense of enclosure and intrusion creating an overwhelming and uncomfortable environment. Adequate tower separation enhances opportunities for sky views and creates a feeling of openness that enhances comfort and livability.

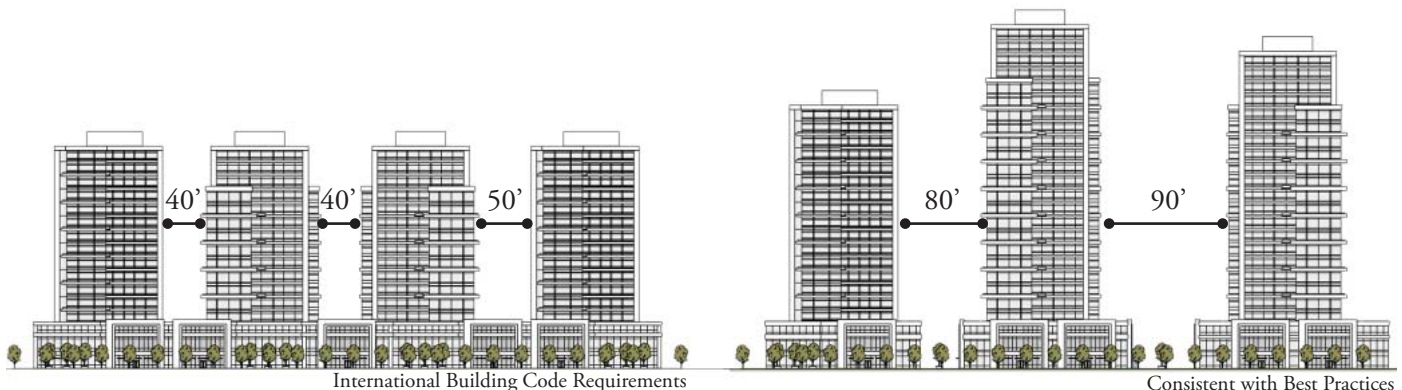


Figure 2.1 - Tower Separation Elevation

Design Excellence

Tower separation requirements can enhance the placement of multiple towers on a single site and can create opportunities for abstraction and uniqueness in form. Added visual interest and variation can allow building forms and massings to create fluidity in design, resulting in a more aesthetically pleasing form and skyline as a whole. Towers can become more expressive and offer variation from a more traditional rectilinear form.

Building Performance and Conditions

Adequate tower separation can improve opportunities for daylight internal to buildings. While improving the quality of life of residents and users, daylighting is critical to sustainable building practices. Inadequate tower separation increases the amount of shade and shadow cast upon adjacent buildings, increasing the reliance of artificial lighting. This diminishes the quality of the internal space while reducing building efficiency.

Precedent

Tower separation has become an important consideration to many urban environments. This separation is to ensure access to light, air, and design excellence within an urbanizing environment. Some examples are as follows:

- **San Francisco**
 - Minimum Separation: 115'
 - Beginning Height: 85'
- **Toronto**
 - Minimum Separation: 82' (25 meters)
 - Beginning Height: 40'
 - Exception made for small sites
- **Honolulu (TOD Overlay)**
 - Minimum Separation: 80'
 - Beginning Height: Required for all towers below 240' in height
- **Vancouver, BC**
 - Minimum Separation: 80'

CAC References

Downtown Livability Initiative - Pg. 45

Land Use Code Reference

20.25A.020.A.2

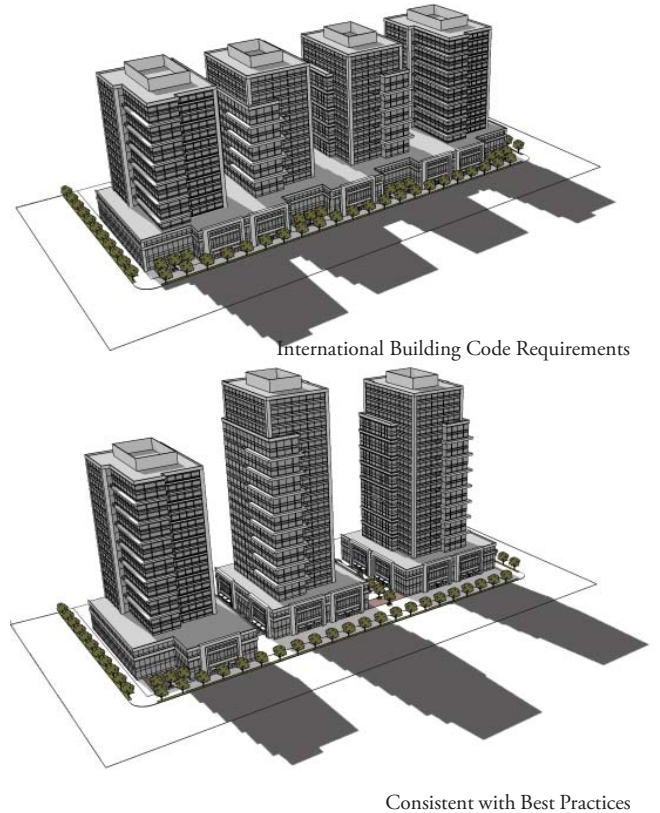


Figure 2.2 - Combined with increased building height, tower separation requirements can reduce the total number of towers per site accommodating the same FAR while, mitigating impacts of shade and shadow on the public realm.

Why it is Important

Floor plate size can have a profound impact on shade, shadow, sky views, and project feasibility. If the scale of a floor plate is too large it can cast important public spaces and the pedestrian realm into permanent shade or diminish opportunities for skyviews. Additionally, large floor plates can create an imposing feeling on the pedestrian realm, impacting the sense of comfort of the urban environment. If the scale of a floor plate is too small it can make the project no longer economically or structurally feasible creating a restraint on the development market. Establishing a balance is essential to preserving quality of life for residents and businesses, while ensuring feasibility for developers.

The determining factors and existing code for floor plates are as follows:

Commercial Office Towers

Based on existing research by the Urban Land Institute, Bellevue's maximum floor plate sizes for office buildings is competitive with industry preferences. Current code allows for up to 24,000 square feet above 80 feet with provisions for increases when deemed appropriate. The City may consider alternatives that influence form to produce more desirable outcomes that are amenable to a high quality urban environment. Limiting facade lengths could mitigate the building scale while improving design, access to sunlight, and the impact of shade and shadows. Seattle, San Francisco, and Vancouver are just three cities that have established limitations on the overall length of any single facade.

The maximum facade length should be influenced by the existing maximum floor plate size as well as current trends in commercial office space development. Based upon ULI's research a five foot by five foot grid is optimal in producing usable and functional office space. As such, any recommendations for maximum facade length, modulation requirements, or other measures to diminish the visual scale of a tower floor plate should use the five by five grid as a guiding principle.

Residential Towers

Residential towers desire smaller floor plate sizes and present the greatest opportunity to capitalize on an increase in building height. Current floor plate sizes are allowed up to 20,000 square feet between building heights of

40 and 80 feet. Above 80 feet the maximum floor plate size is 12,000 square feet. As residential buildings have greater flexibility in layout, there is an opportunity to produce improved design quality by incorporating minor building setbacks for heights above the existing building height as well as maximum facade lengths.

Connecting Floor Plate Provision

Land Use Code 20.25A.020.B.3

The connecting floor plate provision has allowed buildings under 70 feet in height to exceed the maximum allowable floor plate size under the premise that building exiting patterns and construction costs are more efficient by creating a more contiguous form. This provision is intended to work with low rise buildings within the Downtown. The existing provisions call for the following

- The connection is to allow for safe and efficient building exiting patterns. The connecting floor area shall include required exiting corridor area and may include the floor area of units or other building uses.
- The connection occurs on no more than three floor levels above 40 feet.
- The alternative design results in a building mass that features separate and distinct building elements.

When considering the connecting floor plate provision, issues of mass, scale, and public space are essential. Existing maximum floor plate size requirements are intended to

protect, preserve, and enhance these elements and should be preserved when an exception is allowed. The results of this provision have provided an opportunity for development limitation to circumvent the intention of floor area ratio (FAR), which was to limit the scale of an entire massing and its relationship to the total site area. In lieu of providing open space to the public or design excellence that would create a more amenable streetscape to pedestrians, an out of scale massing has typically been produced with “open space” internalized to the building. As such, the scale of the massing is not reflective of the intents of the FAR limitations.

CAC References

Downtown Livability Initiative - Pg. 45

Land Use Code Reference

20.25A.020.A.2

20.25A.020.B.3



Figure 3.1 - Built Example - Positive Result



Figure 3.2 - Built Example - Negative Example



Figure 3.3 - Built Example - Negative Example

Why it is Important

Prolonged shade and shadow can have a detrimental impact on outdoor public spaces and natural light within buildings. Bellevue’s public spaces are an asset for recreation, gathering, and other activities. As such, ensuring that public spaces receive an adequate to abundant amount of sunlight and sky views is important to protecting their value and functionality within the greater network of Downtown’s public spaces. Furthermore, proper consideration can ensure that the users of private spaces within buildings will have appropriate access to sunlight as well. Currently shade and shadow analysis is guided by the comprehensive plan and when a project triggers and Environmental Impact Analysis. The Land Use Code does not provide firm guidance regarding shade and shadow.

Tower Orientation

Positioning a building so that the largest façade is oriented north-south can result in large shadows impacting the pedestrian realm and other adjacent buildings. Orienting a building so that largest of facades faces east-west can help mitigate these impacts.

Tower Spacing

Appropriate separation of towers increases opportunities for sunlight to penetrate the pedestrian realm and street, breaking up the cumulative impact of shadows from multiple towers. Towers spaced too closely together can result in an overabundance of shadows limiting opportunities for sunlight to reach street level and lack of natural light within internal building environments.

Tower Placement

The location of a tower on a site can create adverse conditions for shadow and permanent

shade. Locating large facades and public spaces on the same north-south axis can create an inhospitable public space. Tower placement should avoid aligning with public spaces and other significant pedestrian oriented spaces to allow for sunlight to access the space. This is essential to encourage the use and activation of pedestrian oriented spaces.

Tower Form

Building forms that are rigid in rectilinear form limit opportunities to mitigate shade and shadow. Fluid forms that include tapering, angles, and curves can allow the scale and length of a façade to diminish reducing its overall impact on the pedestrian realm.

CAC References

Downtown Livability Initiative - Pg. 66

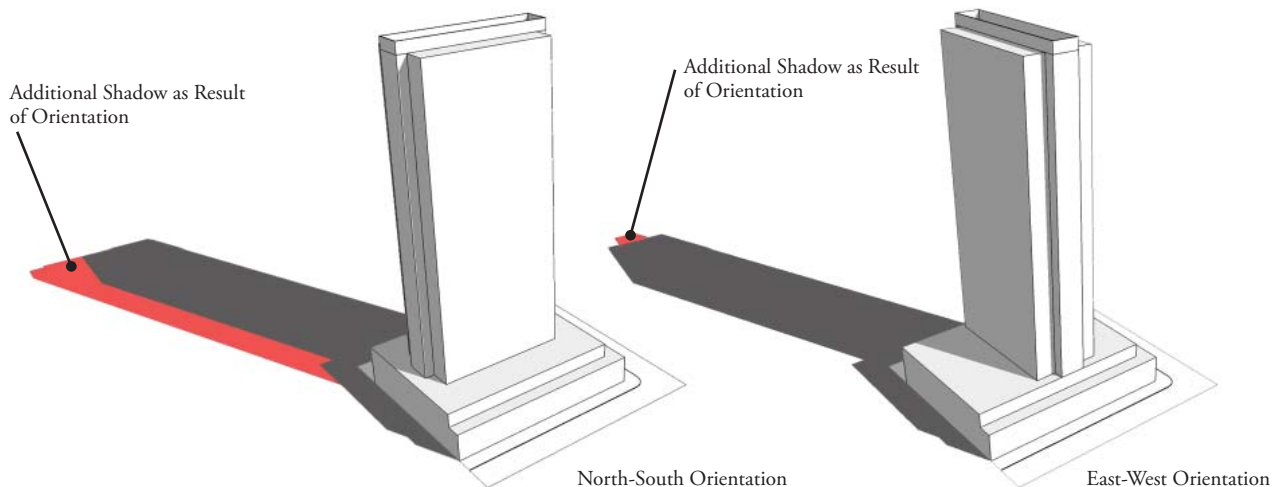


Figure 4.1 - Shadow Results per Orientation

Why it is Important

In fall and winter months wind can be a significant factor in the quality and experience of the public realm. Towers play a critical role in this experience, either mitigating the impacts of wind or accelerating and exaggerating them. Issues such as placement, form, and modulation can assist in preventing wind tunnels or other adverse conditions at the street level. Additionally, treatments to facades at the street level can assist in mitigating the impacts of wind. Downtown's prevailing winds come from the south and southwest. The Comprehensive Plan provides guidance on mitigating wind impacts but does not provide firm recommendations

Determining factors of wind impacts:

Facade Length and Orientation

Orienting the largest facades in the direction of prevailing winds can create a condition where winds impact the facade and accelerate to the pedestrian level, known as down draft. This can have significant adverse impacts on plazas and other public spaces making them unfavorable. Orienting the most narrow of facades in the direction of the prevailing winds can mitigate this issue.

Tower Stepback

Towers that directly interface with the street level and are devoid of an adequate setback or podium can result in down draft. Without a stepback, winds impact the facade and accelerate to the pedestrian level. Providing a setback for the tower massing creates an opportunity to interrupt the down draft prior to reaching the pedestrian level.

Additionally, a tower setback can resolve circulation issues between buildings. Without a stepback wind can accelerate and then recirculate between buildings creating unfavorable pedestrian conditions. Providing a green roof or other form of vegetation on a stepback roof can further mitigate the impacts of downdraft.

Pedestrian Level Treatment

Several building elements can prevent down draft or tunneling between buildings, resulting in improved public spaces and pedestrian conditions.

- *Marquees and Canopies* - While not as effective as a building stepback, marquees and canopies can create interruptions of down draft. This can be particularly effective for shorter or narrower towers where an adequate setback is not always feasible. They can also offer mitigation where a setback does not occur on all sides of a building
- *Arcades* - Arcades can offer shelter from downdraft where a setback is not possible. Arcades should be used where appropriate separation can occur such as along a street or public plaza. Utilizing an arcade where appropriate separation is not provided can result in tunneling, which can further exaggerate the impacts of wind at the pedestrian level.
- *Vegetation* - Trees and other vegetation in public spaces can help interrupt down draft and tunneling where other measures have not been as successful. Vegetation should only be used as an addition and not as the only source of mitigation for down draft or tunneling.

CAC References

Downtown Livability Initiative - Pg. 66

Issue

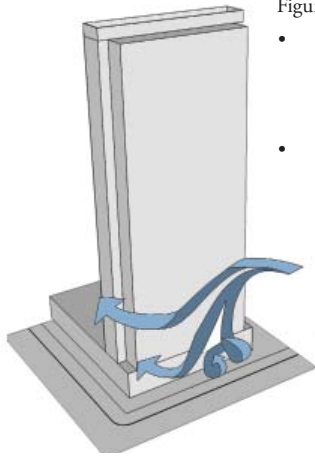


Figure 5.1 - Down Draft Issue

- Tall and wide facades oriented in the direction of the prevailing winds can exaggerate the effect of wind on the pedestrian realm.
- An uninterrupted middle to base facade can create adverse wind conditions at street level.

Resolution

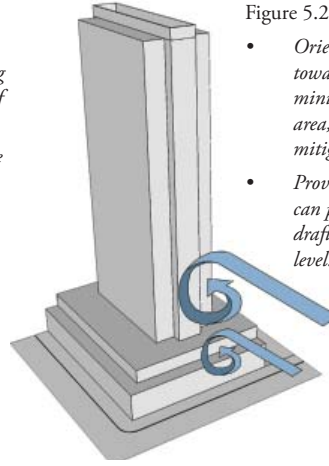


Figure 5.2 - Down Draft Resolution

- Orienting the most narrow facade towards the prevailing winds can minimize the amount of surface area, creating opportunities to mitigate the impacts of down draft.
- Providing an adequately stepped base can provide interruption in down draft, improving conditions at street level.

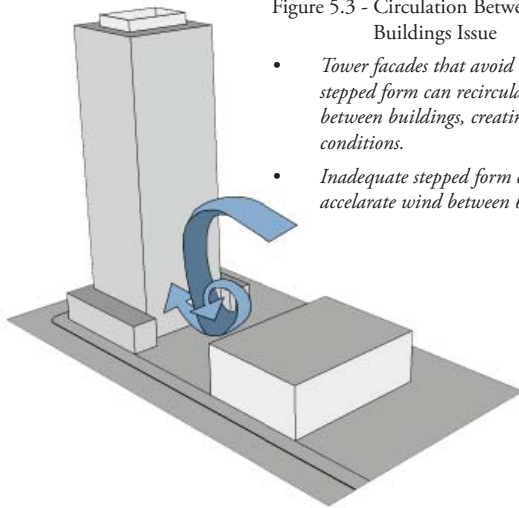


Figure 5.3 - Circulation Between Buildings Issue

- Tower facades that avoid a stepped form can recirculate wind between buildings, creating adverse conditions.
- Inadequate stepped form can accelerate wind between buildings.

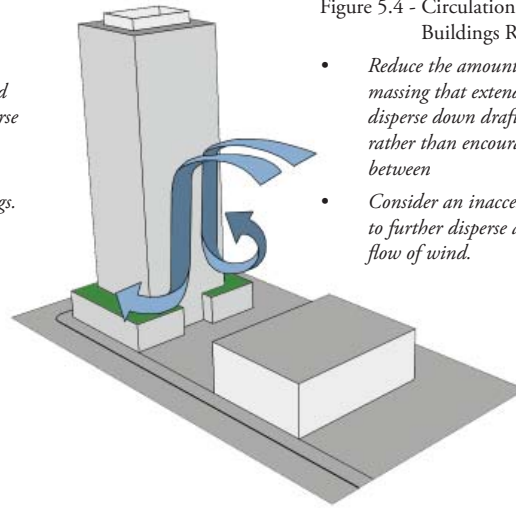


Figure 5.4 - Circulation Between Buildings Resolution

- Reduce the amount of the tower massing that extends to street level to disperse down draft around buildings rather than encourage circulation between
- Consider an inaccessible green roof to further disperse and mitigate the flow of wind.

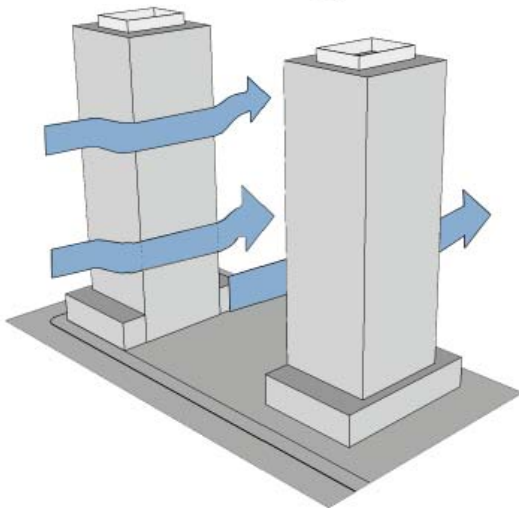


Figure 5.5 - Wind Tunneling Issue

- Wind tunneling between high rises can occur when setbacks and other building elements are absent that could create opportunities to divert wind flow.
- Height, spacing, and orientation can all affect wind speed and direction.

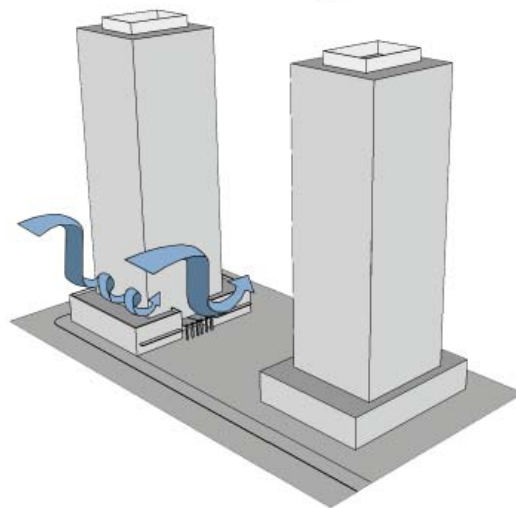


Figure 5.6 - Wind Tunneling Resolution

- Parapets, canopies, and arcades can all create opportunities for wind diversion, slowing wind speeds and mitigating negative impacts on the pedestrian realm.
- Consider opportunities for variations in height, placement, and facade articulation to prevent wind acceleration and tunneling.

Why it is Important

The quality of the environment of the public realm is highly dependent on the physical form of surrounding buildings. In addition to the physical and environmental impacts of tower design, they also possess a great opportunity to establish civic pride, memorable views, and landmarks. Tower design can create emotional and aesthetic responses for residents, employees and visitors that can create a lasting and memorable impression of a city or neighborhood. Using opportunities to address physical and environmental conditions, timeless and memorable buildings can be created. Classic design principles can be used to inform modern design to create a memorable building and form.

Important elements to building form and design:

Base - Middle - Top

Base

The base of the tower represents the greatest opportunity for a tower to relate to the human scale, and allow for a tower to interface gracefully with and contribute to the pedestrian realm. Additionally, the base is the critical connection between the public realm of the street and the private realm of the building. The base should provide a sense of enclosure while offering adequate variation and interest that engages pedestrians. This variation can be achieved through points of interest, glazing, and entrances. The height of the base should have a relationship to the width of the right of way or to surrounding buildings for consistency and to create a comfortable sense of enclosure.

As a pedestrian's dominant experience occurs within the first two stories, design of the base can help define the pedestrian realm and create an engaging environment. In areas adjacent to sidewalks, maintaining a strong building facade is important to help define the pedestrian realm and increase opportunities to create an engaging and active pedestrian environment. This can be done by bringing the building facade up to the edge of pedestrian areas to create a building facade that directly engages with the pedestrian realm. Within the strong building facade, articulation and modulation

of the building face adds interest to the pedestrian realm.

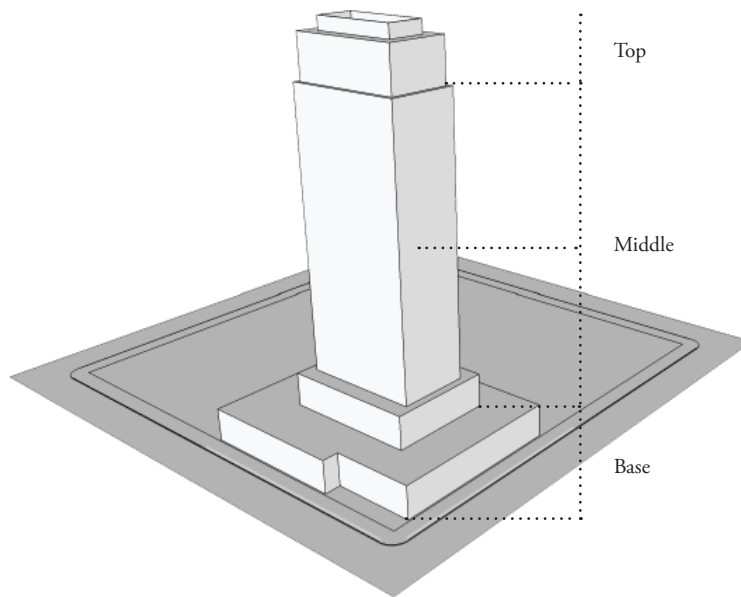
Design details, such as materials used on the building facade, door and window fenestration, and other forms of ornamentation add regular and semi-regular interest. Materials used on the building facade within the pedestrian realm should be scaled to the pedestrian. Where required and also in high pedestrian areas, weather protection is used to both promote year round use of pedestrian-activated-frontage and to help define the upper edge of the pedestrian realm.

Middle

The middle of the tower constitutes primary massing. To emphasize the base/middle/top principle the middle of the tower should step back from the base or podium, providing clear delineation between the human scaled nature of the base and the overall massing of the middle. As the primary massing of a tower, floor plate size and separation are critical in protecting views and sunlight while creating a form that provides architectural interest. When these elements are executed appropriately a tower can achieve design excellence while establishing an elegant and visually stimulating skyline.

Top

The top of a tower can create opportunities to further design excellence and visual interest while screening necessary building equipment such as elevator overruns and mechanical equipment. Furthermore it allows for greater diminishing of a tower massing by creating opportunities for stepbacks and building tapering that provides a more elegant and refined termination of a building form. When executed appropriately the top of a tower can serve as a signature and defining element in a skyline.



Why it is Important

Preservation of views of our iconic natural landmarks from public places is necessary to both maintain the character of Bellevue as a “City in a Park” and to provide visual access of our most iconic natural resources to the public.

The Process

City of Bellevue staff surveyed Downtown for key views of Mount Rainier and other landmarks from public places. Through this review it was determined that the only significant view from a public place in Downtown Bellevue was from City Hall to Mount Rainier. While other views exist of Mount Rainier from other public places, partial obstructions prevent clear views.

To analyze the view of Mount Rainier from the Bellevue City Hall concourse balcony, staff employed internal professional land surveyors to identify and illustrate the desired viewing window. The viewing window, represented in the provided plan, was identified by surveying the width of the view of Mount Rainier as it hits the Newcastle horizon and adding this width to each side of the mountain to allow for minimum territorial context. The bottom of the viewing window was established through the survey by establishing the lower western horizon elevation as the base elevation for the viewing window. This was taken from the elevation of the concourse balcony at 153.12 feet plus the height of the measurement mechanism of 5.61 feet giving a total elevation of the origin of the view at 158.73. A survey pin has been set on the concourse balcony indicating the origin.

The plan (Figure 7.2) showing the cone of vision from City Hall to Mount Rainier has five lines: four dashed and a center line. The center line represents the peak of the view of Mount Rainier. The dashed lines on each side of the red line indicate the points where the edge of the view of Mount Rainier hits the horizon, in this case Newcastle. The outer dashed lines represent the width of the view of Mount Rainier itself added to each side of Mount Rainier to add territorial context to the view.

The elevation (Figure 7.3) provided shows the viewing window in elevation for demonstration purposes. Heights shown provide an idea of the allowable height of a building in that specific location along the parcel boundary in order to not obstruct the view of Mount Rainier. Topography changes within parcels create further height variation.



Figure 7.1 - View of Mount Rainier from City Hall Concourse

- To add territorial context to the view, surveyors added the width of the view of Mount Rainier as it hits the Newcastle skyline to each side of Mount Rainier.

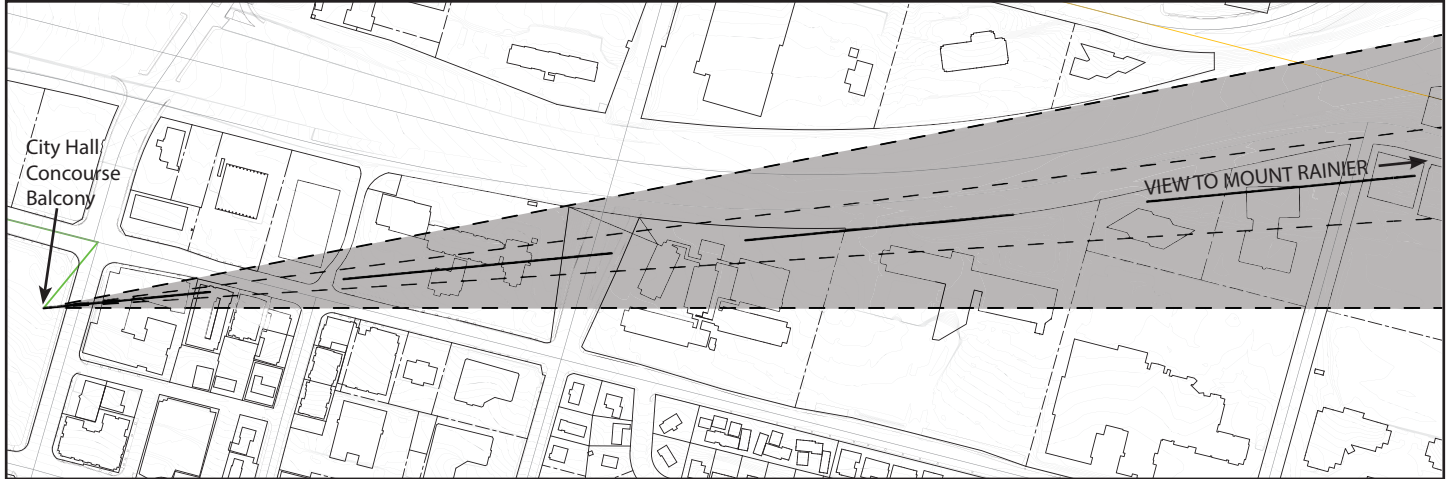


Figure 7.2 - Plan of Preserved View of Mount Rainier

- The center line represents the peak of the view of Mount Rainier. The dashed lines on each side of the red line indicate the points where the edge of the view of Mount Rainier hits the horizon, in this case Newcastle. The outer dashed lines represent the width of the view of Mount Rainier itself added to each side of Mount Rainier to add territorial context to the view.

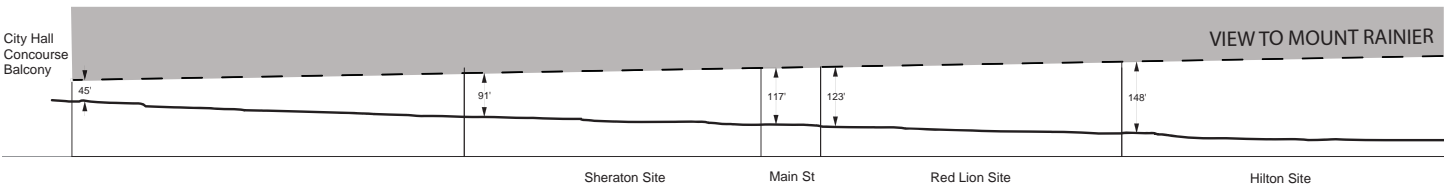
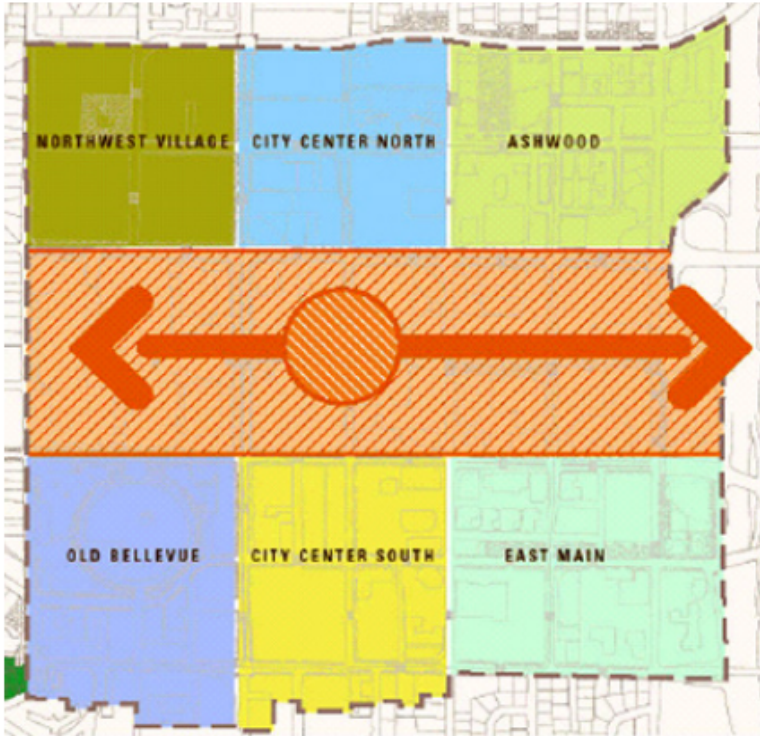


Figure 7.3 - Elevation of Mount Rainier View

- The elevation shows maximum building heights on the underlying parcels in order to not obstruct the view of Mount Rainier. Current height limits are not impacted by this protection.

Neighborhood Identity Intent Statements



a. Northwest Village

Northwest Village is intended to have an “alleys with addresses” character by focusing activity on internal streets and through-block connections. Land uses are primarily residential with substantial neighborhood serving services (e.g., grocery). This calls for family and children-oriented, small-scale open spaces with active play areas. Interconnected, meandering walkways and alleys, faced with restaurants, shops, services, and residential entries, would provide a safe, lively, and connected network of linear open spaces. A neighborhood park in the vicinity of NE 10th Street and 102nd Ave NE intersection and connected to the pedestrian network would provide a central place for active play and social gathering. Building and tower separation should provide for substantial light and air for sky views. Special attention should be given to providing a sensitive transition to neighborhoods outside Downtown.

b. City Center North

City Center North includes the northern terminus of (106th Ave NE the event and entertainment street of Downtown and the home of the future Tateuchi Performing Arts Center. An activated plaza for outdoor performances, joint functions, and social gathering near 106th Ave NE and NE 10th Street would complement the performing arts center and entertainment focus. In this district features a number of high-rise housing complexes. It is important that development reinforce its identity as in the district as a high quality urban place to live. Residential amenities, such as pocket parks and outdoor recreation, throughout the neighborhood would strengthen that character.

c. Ashwood

Ashwood is a predominantly multi-family district that is an ideal location for civic and neighborhood uses that encourage and support pedestrian activity. Ashwood Park, Kids Quest, and the King County Library create a strong civic core in this neighborhood. While Ashwood Park provides an essential recreational asset, facilities such as a children’s play area, outdoor pet area, community garden, and others as identified by the local neighborhood are needed. Some of these may be added at Ashwood Park; others will occur at scattered locations in the vicinity. The residential focus with ground level entries and family-sized housing are a priority. Pedestrian and bicycle connectivity including mid-block crossings enhance the livability of this district.

d. Eastside Center

The Eastside Center is intended to be the location for the most intense and pedestrian activated development. Uses range from destination shopping to transit oriented development along I-405. This area includes all signature streets; The Shopping Street – Bellevue Way, The Entertainment/Event Street - 106th Ave NE, and The Commerce Street – 108th Ave NE. - The 6th Street Pedestrian Corridor and future Grand Connection across I-405 to Wilburton connects the Bellevue Transit Center and Light Rail Station, Meydenbauer Convention Center and Theater to Bellevue Square through a mostly office and commercial area. This Corridor, Compass Plaza, and the other accompanying open spaces are key defining elements for the entire Downtown. As the Downtown Core, Pedestrian Corridor, and connections to it evolve, more outdoor activities should be integrated to encourage use through-out the day and year such as space for active uses, such as children’s play areas, sports courts, and small plazas with active edges. Unifying elements on the Corridor should come from the Art Walk and Grand Connection may include timeless and kinetic art, green infrastructure, and bicycle amenities (discussed further in the Pedestrian Corridor Design Guidelines).. Exceptional pedestrian and bicycle access to transit and light rail should be a priority.

e. Old Bellevue

Old Bellevue’s character is largely defined by the lively pedestrian and boutique nature of the neighborhood as well as Downtown Park, proximity to Meydenbauer Bay, and the Lake-to-Lake Trail on Main Street. Priorities include improved connectivity between them. These connections should offer landscaping and streetscape amenities that support safe, lively, and comfortable routes. To support the Lake-to-Lake Trail concept and reinforce Main Street as an attractive stopping point for cyclists, special bicycle facilities, and a safe biking environment should also be a priority. Adequate space for sidewalk cafes, community events, and lively, multi-use urban space should be provided for sidewalk cafes, community events, and the future Grand Connection and Art Walk routes. Development should reflect the intimate scale and historic feel of this vibrant visitor attraction and gateway to Downtown.

f. City Center South

City Center South is intended as a mixed-use neighborhood and mid to high-rise development is appropriate. Streetscapes with green infrastructure are a high priority throughout the district and especially along Main Street and NE 4th Street. Main Street calls for wide sidewalks and special bicycle facilities as part of the Lake-to-Lake Trail 108th Ave NE and Meydenbauer Bay and provide the opportunity to generate a green and water-oriented theme particularly for Main Street. The linear buffer that separates Downtown from the neighborhood to the south should function as green infrastructure that emphasizes the movement and treatment of water while buffering, educating, and entertaining pedestrians and cyclists. Because substantial large parcel redevelopment is likely, there will also be the opportunity to create a variety of open spaces along the through-block pedestrian connections. This area should continue to provide neighborhood serving retail and a transitional scale, uses, and materials responding to neighborhoods to the south.

g. East Main

East Main district is a neighborhood intended to have a mix of residential, office, and retail due to its proximity to the light rail. Significant public open spaces at 110th Ave NE and NE 2nd Place and a park along Main Street between 112th and 110th Aves NE that lids the light rail tunnel portal. Considering the fairly dramatic topography in the area, terracing is an appropriate identifying feature of open spaces in this area. As a neighborhood with strong residential and office components, multi-use public areas that serve different types of users throughout the day are especially important. In addition, recreational uses, especially those that can make use of the topography, are important for enhancing neighborhood qualities. Because substantial large parcel redevelopment is likely, there will also be the opportunity to create a development with variable heights that provide for light, air, and open spaces along the through-lock pedestrian connections. Exceptional pedestrian and bicycle access to the East Main Light Rail Station is a priority. This area is especially appropriate for transit oriented development. The Lake to Lake Trail continues along Main Street. Widened sidewalks with a mix of adjacent pedestrian amenities is a priority.

Building/Sidewalk Right-of-Way Designations

ROW Designation	Ground Floor Frontage	Visual & physical access	Weather protection ¹	Entry or other major point of interest ²	Parking & Vehicular Access
A Pedestrian Corridor/High Street Most intensely pedestrian activated streets	100% PAF ³ 13-15 ft min flr to clng ht. <u>16 ft flr/flr min.</u>	75%	75%	Every 60 ft. max	None, except where no other option available. <u>No surface parking or vehicle access directly between sidewalk and main pedestrian entrance.</u>
B Commercial Streets Streets in Core with a balance of retail and other uses	50% PAF min; 50% service ⁴ , 13-15 ft min flr to clng ht. <u>16 ft flr/flr min.</u>	75%	75%	Every 75 ft. max	Yes, <u>but with limitations.</u> <u>No surface parking or vehicle access directly between perimeter sidewalk and main pedestrian entrance.</u>
C Mixed Streets Streets outside the Core that accommodate a variety of uses	Developer choice – mix of PAF, service, office, residential, and green walls;	75%	75%	Every 90 ft max.	Yes, with limitations. <u>No surface parking or vehicle access directly between perimeter sidewalk and main pedestrian entrance.</u>
D Neighborhood Streets Streets outside the core with residential and neighborhood services focus	13-15 ft min. flr to clng ht. <u>16 ft flr/flr min.</u> <u>14 ft. flr/flr min. for ground floor residential</u>	50%	50%	Every 90 ft max.	<u>No surface parking or vehicle access directly between perimeter sidewalk and main pedestrian entrance.</u>
E Perimeter Streets	Developer choice – mix of PAF, service,	50%±	50%	<u>Every 90 ft max.</u>	Primary access off streets not facing residential neighborhoods unless no other option available

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¹ See new LUC 20.25A.060.B Overhead Weather Protection for design criteria.

² Major Points of Interest: An element such as a large landscape feature, event space, art, water feature, open space, and through-block connection, residential unit entries and courtyards.

³ Pedestrian- Activated Frontage (PAF): Retail and personal services that general pedestrian activity including retail stores, groceries, drug stores, shoe repair, cleaning, floral, barber and beauty services, art galleries, travel agencies, eating and drinking establishments, and theaters.

⁴ Service: A range of personal and professional services including, finance, insurance, real estate, and business services. Design for these uses are intended to be pedestrian-attracting in nature.

ROW Designation	Ground Floor Frontage	Visual & physical access	Weather protection	Entry or other major point of interest ²	Parking & Vehicular Access
Streets with a neighborhood focus and scale or transition to neighborhoods.	office, residential, and green walls. 13-15 ft min. flr to clng ht. <u>14 ft. Flr/flr min. for ground floor residential</u>				

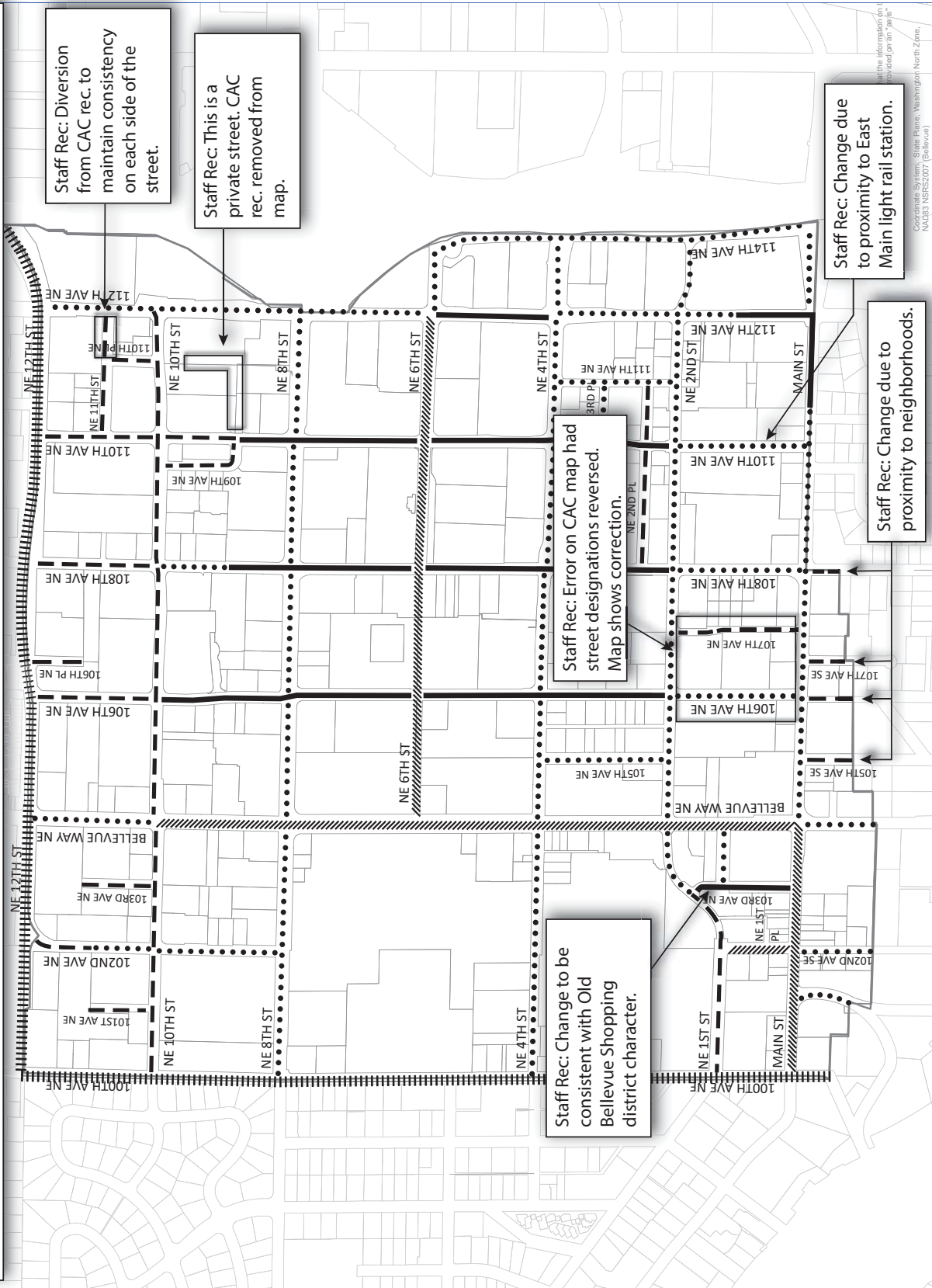
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Staff recommendations follow CAC recommendations (pg. 32 of CAC report) with a small number of suggested refinements as noted below:



Coordinate System: State Plane, Washington North Zone, NAD83 NSRS2007 (Bellevue)



Building Sidewalk Relationship Guidelines

Legend

-  A - Pedestrian Corridor / High Street
-  B - Commercial Street
-  C - Mixed Street
-  D - Neighborhood Street
-  E - Perimeter Street
-  Parcels
-  Downtown Boundary



600 Feet

Sources: City of Bellevue

Design of Through-block Connections

Selection of frontage type at developer's discretion

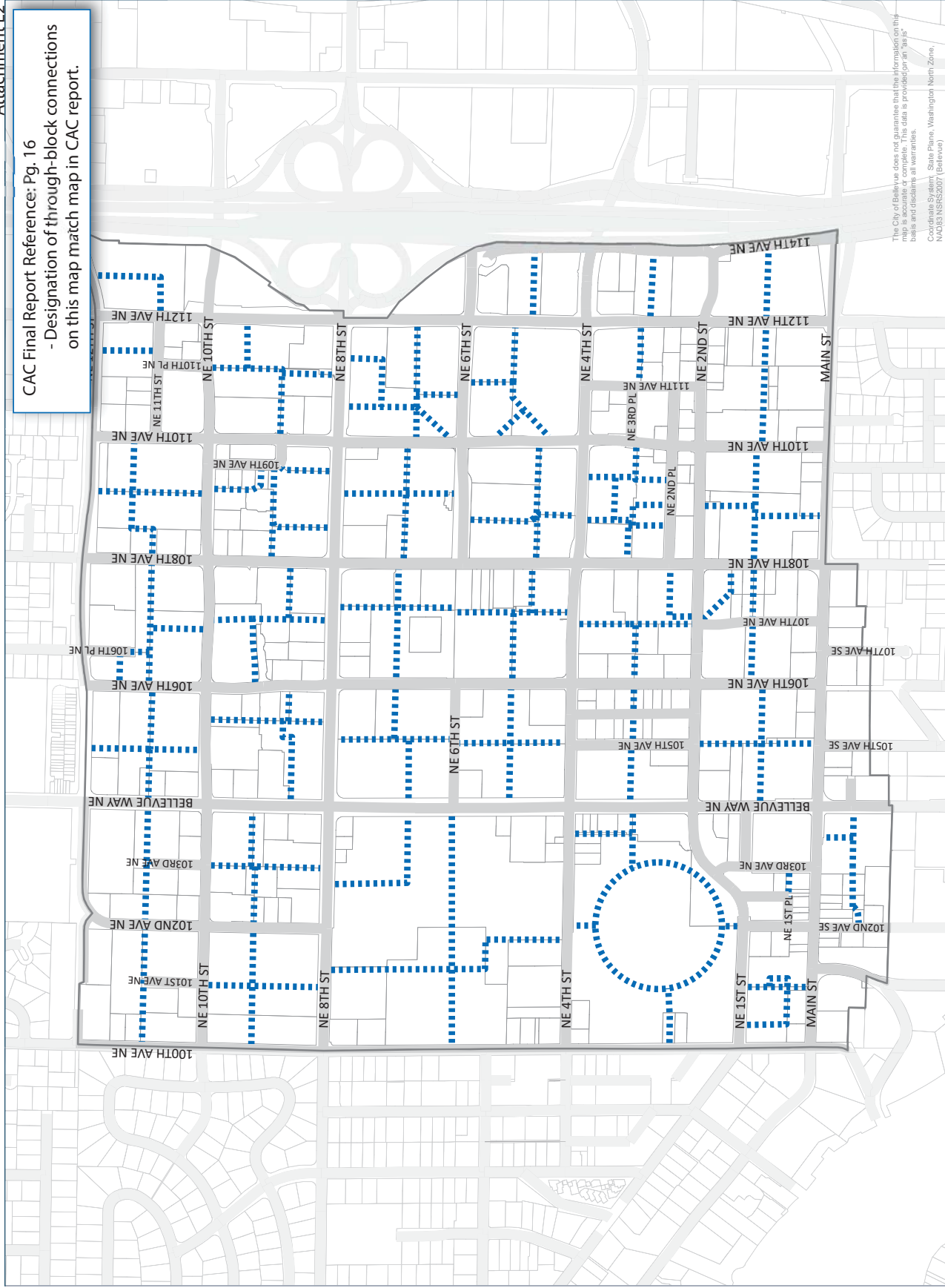
Frontage	Description	Standard
Retail Connection	Retail storefronts with generous window transparency, pedestrian entries, weather protection, and outdoor seating/dining area	12 ft. clear minimum
Residential Activations	Stoops or similar frontages with private individual entries, private individual patio frontages, lobbies/common residential entries or other common facilities with generous transparency/activation elements	6 ft. clear minimum <u>8 ft. preferred</u>
Passive/Walk-throughs	Passive corridors featuring landscaping, lighting human scaled details, and other pedestrian amenities.	6 ft. clear minimum <u>8 ft. preferred</u>
Vehicular plus Pedestrian Access	Low traffic route where autos and pedestrian share space or separated access. Lighting, landscaping and other design elements separate autos from pedestrians to create a safe and attractive pedestrian route. Frontages should be landscaped or provide transparency with human scaled details for visual interest.	6 ft clear minimum for pedestrian. Vehicular TBD by access requirements. <u>8 ft. preferred</u>
Through-building connection (project specific)	<u>Appropriate treatment determined through Design Review</u>	<u>Varies</u>

Commented [WP1]: Staff recommendation. Added for clarity/flexibility.

Commented [WP2]: Staff recommendation. 8 ft preferred for adequate pedestrian mobility and walkability (typ).

Commented [WP3]: Staff recommendation. Added for clarity.

CAC Final Report Reference: Pg. 16
- Designation of through-block connections
on this map match map in CAC report.

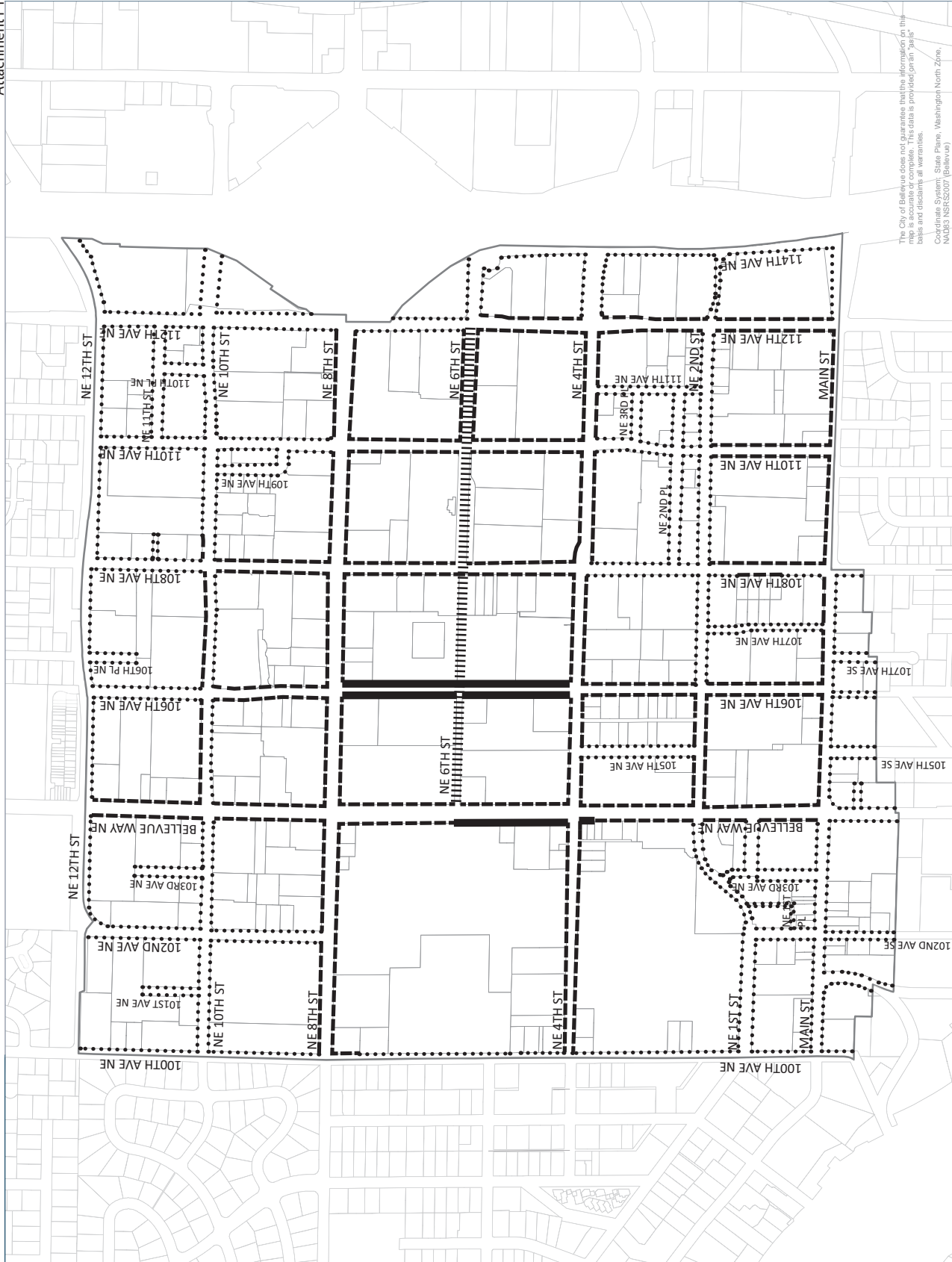


The City of Bellevue does not guarantee that the information on this map is accurate or complete. This data is provided on an "as is" basis and disclaims all warranties.
Coordinate System: State Plane, Washington North Zone, NAD83 NRSR2007 (Bellevue)
IT Department

Through-Block Connections

- Legend**
- Through-block Connection
 - Downtown Boundary
 - Streets
 - Parcels

600 Feet
Source: City of Bellevue






The City of Bellevue does not guarantee that the information on this map is accurate or complete. This data is provided on an "as-is" basis and disclaims all warranties. Coordinate System: State Plane, Washington North Zone, NAD83 NSRS2007 (Bellevue)

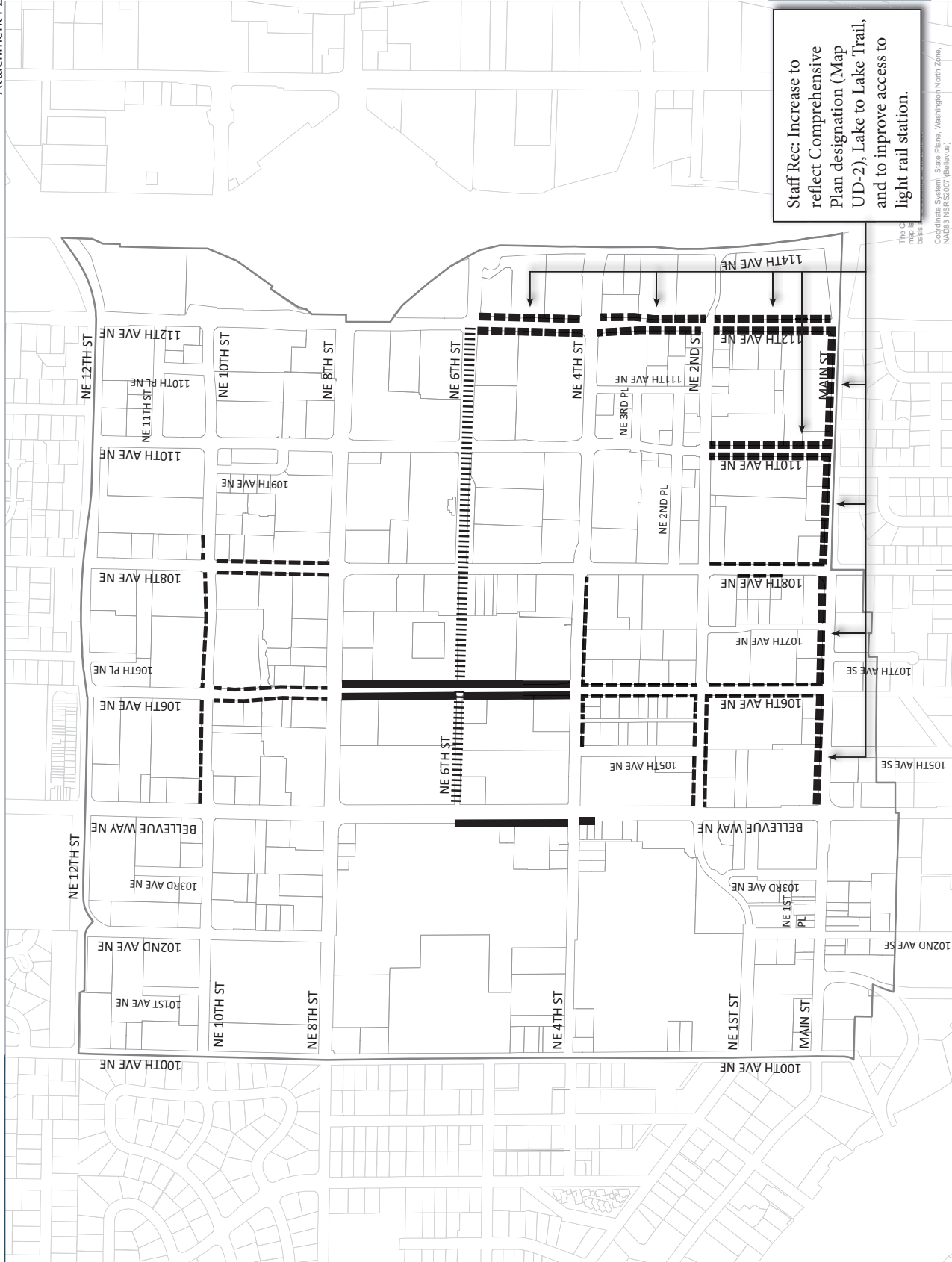


Downtown Sidewalk Dimensions

- Legend**
- 12' Sidewalk Width
 - - - 16' Sidewalk Width
 - 20' Sidewalk Width
 - ||||| Pedestrian Corridor
 - Parcels
 - Downtown Boundary

Sources:
City of Bellevue




Staff Rec: Increase to reflect Comprehensive Plan designation (Map UD-2), Lake to Lake Trail, and to improve access to light rail station.

The City of Bellevue
 Coordinate System: State Plane, Washington North Zone, NAD83 NSRS2007 (Bellevue)



Downtown Sidewalk Dimensions Changes

- Legend**
- Downtown Transportation Plan Changes
- Increase from 12' to 16' Width
 - Increase from 16' to 20' Width
 - Staff Recommendations
 - Increase from 12' to 16' Width
- ||||| Pedestrian Corridor
 — Downtown Boundary



600 Feet

Sources: City of Bellevue



Planning Commission Schedule

January 13, 2016

The Bellevue Planning Commission typically meets on the second and fourth Wednesdays of each month. Meetings begin at 6:30 p.m. and are held in the Council Conference Room (Room 1E-113) at City Hall, unless otherwise noted. Public comment is welcome at each meeting.

The schedule and meeting agendas are subject to change. Please confirm meeting agendas with city staff at 425-452-6931. Agenda and meeting materials are typically posted no later than the Monday prior to the meeting date on the city's website at:

<http://www.bellevuewa.gov/planning-commission-agendas-2016.htm>

<u>Date</u>	<u>Tentative Agenda Topics</u>
Jan 27, 2016	Eastgate Land Use Code
Feb 10	Downtown Livability
Feb 24	Eastgate Land Use Code
Mar 2	State Department of Commerce – Short Course on Local Planning (hosted by the City of Bellevue)
Mar 9	Downtown Livability Annual Comprehensive Plan Amendments
Mar 23	Eastgate Land Use Code Single Family Room Rental

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% RADFORD & CO
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BELLEVUE, WA 98004

Review of Dec 9, 2015 drafts of documents

The attachment of the DOWNTOWN legal description looks okay to me.

I support recommendations made during the Dec 9 Public Comment as follows:

Permitted Uses should endure flexibility to allow changing/new uses and convenient drive through pick up for retail and services. Pick up points in and around traffic/parking generators like Bellevue Square might be designed to relieve problems, make life easier for handicapped drivers, moms with kids, etc.

Bonus system should especially reward projects that improve preferred downtown livability and family uses, and introduce additional green spaces, mid-block semi-public walks and squares, shared driveway/parking solutions, public-friendly art.

Public 'pay parking' should be permitted to be built or converted from under used parking structures or lots.

Although light and sound are not under the direct supervision of Design Review, DR should flag potential problems for Administrative Review. Downtown sports courts and other potential night activities should be encouraged, subject to Administrative Review for potential nuisances.

Many thanks to Planning staff and committees for all the hard thoughtful hours spent to update the Comprehensive Plan.

Colin W Radford

On behalf of RAMCO Properties LLC and

Tri Western Syndicated Investments LLC



DATE: January 13, 2016

TO: Chair Hilhorst and Planning Commission Members

FROM: Michael Kattermann, AICP, Acting Comprehensive Planning Manager
mkattermann@bellevuewa.gov, 452-2042
Emil King, AICP, Strategic Planning Manager
eaking@bellevuewa.gov, 452-7223
Planning and Community Development

SUBJECT: Planning Commission Quarterly Check-in – *INFORMATION ONLY*

At the Planning Commission annual retreat in October 2015 there was a suggestion to have quarterly “check-ins” rather than wait until the annual retreat to address topics or questions. In response to that suggestion, staff is scheduling time on the January 13, 2016 agenda to update the status of items raised at the retreat as well as additional feedback from Commissioners in December. In order to make the most efficient use of time, staff is providing this information in this memorandum. There is time on the agenda for the Commission to discuss some of these items at their discretion. If additional time or follow-up is needed, specific items can be scheduled for future meetings.

UPDATE

1. Accomplishments from last quarter – Over the last three months of 2015, the Commission developed a package of Downtown Livability “early win” code amendments. These are anticipated to be forwarded to Council in the January/February 2016 time frame. The Commission’s work on Eastgate included a review of potential land uses within the corridor and a study of concomitant agreements. The Commission also received an update on the Single Family Room Rental ordinance, and asked staff for a follow-up discussion to occur in 2016 based on some implementation concerns.
2. Look at full calendar year work program and identify opportunities for individual commissioners to take the lead or do additional work or research on specific topics or tasks – The coming year is shaping up to be a busy one for the Planning Commission. The work of the Commission in 2016 will include the continuation of land use code amendments for Downtown and Eastgate, Low Impact Development standards, the annual Comprehensive Plan Amendment process, as well as other topics directed by Council. Staff will continue to place a detailed, meeting by meeting, schedule in the Commission packets that looks out a few months ahead. The schedule right now has two meeting per month, on the second and fourth Wednesdays. Based on this meeting frequency, an August break, and associated

holidays, the Commission will meet 20 times over the course of the year in addition to a Commission retreat tentatively calendared for Wednesday, October 19, 2016.

There has been interest from the Commission to potentially have members take the lead or do additional work outside the normal Commission meetings on discrete topics or tasks of relatively short duration. A past example of this was the work of Commissioners Carlson and deVadoss on the Comprehensive Plan vision. With Commission direction, future topics or tasks that might warrant this approach could be assigned to two Commissioners by the Chair and Vice-Chair based on Commissioner interest and/or the need of the Commission to conduct additional work or gain insight on a topic.

3. Increase frequency of retreat “check-in” (e.g. once each quarter) – Staff will schedule these quarterly reports on the Commission’s agenda and work with the Chair to allow time for discussion of items as needed. This will not replace the annual retreat.
4. Clarify protocol for public comment – The Planning Commission bylaws require that all questions, whether from the public or individual Commissioners, be directed to the Chair, who then decides whether to have the question answered at that time or in later follow-up directly with staff. There is no universal protocol for city boards and commissions at this time. The City Clerk’s office is reviewing protocols and bylaws for all boards and commissions with the intent of standardizing them.
5. Provide a way to submit comments directly through the web site and clarify comments will be included in packets – The website has been edited to make it clear that comments received in writing or electronically will be included in the packet for the next scheduled Planning Commission meeting. According to the City’s web master it is impractical to create a separate tool for comments with the existing web site and platform. Bellevue will be launching a new website this summer and staff will explore ways to incorporate a new comment tool with the new site.
6. Resume speaker series and schedule on a quarterly basis – There was interest at the retreat in resuming the Commission’s speaker series from 2013-14. Ray White from Bellevue College attended the Commission’s meeting last September, and March’s upcoming Short Course on Local Planning will include a number of guest speakers. Staff will continue the speaker series in 2016 on roughly a quarterly basis geared towards education of the Commission and public, while supplementing the Commission’s overall work plan.
7. Conduct Commission meetings at other facilities throughout the community – The Commission has expressed an interest in continuing to periodically meet outside City Hall. Picking up from past engagements, the Commission’s September 9 meeting was held at Bellevue College and focused on the Eastgate Area. Moving forward, staff has compiled a list of suitable venues to accommodate the Commission and public and will work with the chair to hold additional meetings throughout the community on about a quarterly basis.

8. Host Planning Commissioner Short Course – Washington State Department of Commerce and City of Bellevue are hosting a Short Course for area planning commissioners on March 2, 2016, from 6 to 9 pm at Bellevue City Hall.
9. Provide City of Bellevue business cards to Planning Commissioners – The City Clerk’s office is preparing business cards for use by the Planning Commission. These will be supplied to you when they are available.
10. Produce a graphic depicting topics and projects being worked on by the Commission – Staff is in process of using a relatively new on-line tool viewable by the public called “story mapping” that will include pertinent map layers and associated annotations regarding the Commission’s work. As needed, staff will also use large-scale hard copy maps to orient the Commission to topics being discussed.
11. Make more use of data – There has been interest expressed in using data. Specifically, the Downtown amenity incentive system has been identified as an example of how data might be gathered, disseminated and analyzed to help direct the work of the Commission and measure progress. Staff will be developing an approach that addresses this interest as the Downtown Livability project continues. In addition on the data front, the City will be rolling out an Open Data Portal on the City’s web site in early 2016, and was also recently selected to participate in the new “What Works Cities” sponsored by Bloomberg Philanthropies.
12. Comprehensive Analysis – There has been interest expressed in further exploring adaptive management and tactical urban processes relative to the Commission’s work. The common elements of these processes are to do something, gain insights to how it’s performing, and make appropriate adjustments. Adaptive management is a key part of the BelRed plan and code, where a defined review period was identified at adoption that will allow for review and refinement as needed. Tactical urbanism typically refers to small, low-cost capital projects that can be easily implemented and analyzed to see how they performed.
13. Televised Planning Commission meetings – At this time only City Council meetings and study sessions are televised on the local cable access channel. There have been previous discussions about whether to televise board and commission meetings; however, due to added cost this has not been done in the past. Staff is discussing this suggestion with the City Clerk’s and City Manager’s offices to determine costs and other implications in consideration of a request for the upcoming 2017-2018 budget.

NEXT STEPS

Staff will continue to work on items that are incomplete or ongoing. Please let us know if there are other items that should be included in the next quarterly check-in.

CITY OF BELLEVUE
BELLEVUE PLANNING COMMISSION
STUDY SESSION MINUTES

December 9, 2015
6:30 p.m.

Bellevue City Hall
City Council Conference Room 1E-113

COMMISSIONERS PRESENT: Chair Hilhorst, Commissioners Carlson, Barksdale, deVadoss, Laing, Morisseau, Walter

COMMISSIONERS ABSENT: None

STAFF PRESENT: Mike Kattermann, Emil King, Patti Wilma, Erika Rhett, Planning and Community Development Department; Carol Helland, Patricia Byers, Development Services Department; Camron Parker, Parks and Community Services Department; Mike Ingram, Transportation Department

COUNCIL LIAISON: Councilmember Stokes

GUEST SPEAKERS: None

RECORDING SECRETARY: Gerry Lindsay

1. CALL TO ORDER

The meeting was called to order at 6:38 p.m. by Chair Hilhorst who presided.

2. ROLL CALL

Upon the call of the roll, all Commissioners were present.

3. APPROVAL OF AGENDA

A motion to approve the agenda was made by Commissioner Laing. The motion was seconded by Commissioner Carlson and the motion carried unanimously.

4. PUBLIC COMMENT

Mr. Todd Woosley, PO Box 3325, spoke representing the Kramer family, owners of the RV park in Eastgate. He urged the Commission to continue moving the issue forward to avoid the risk of missing the boom part of the boom/bust cycle of the market. With regard to concomitant agreements, he said it would be good to clean them up as they may limit the development of existing properties. However, trying to undo all of them could become a quagmire taking quite a while. One option would be to get the rezone in place ahead of working out the details of the concomitant agreements. Some of the goals of the Eastgate/I-90 plan are called out in the staff memo, including revising building heights and the allowed FAR. In moving forward, things should get done in a timely fashion.

Commissioner Carlson asked where things stand relative to the boom/bust cycle. Mr. Woosley said the forecast through 2016 is for the market to remain very strong. However, if there are four

quarters to the game, it is likely the game is in the third quarter. Rents are continuing to rise, absorption is strong, there is a supply and jobs keep getting created. All things change, though, and as they do the funding for additional housing, even where the need is high, could be jeopardized.

Mr. Carl Vander Hoek, 3421 2nd Avenue SE, expressed an interest in having the Planning Commission meetings broadcast on Bellevue TV just as the City Council meetings are. He said he has heard from members of the community that they would be much more participatory in the process if the meetings were on TV. In time all boards and commissions should be broadcast. The residents and the business community would be very grateful.

5. COMMUNICATIONS FROM CITY COUNCIL, COMMUNITY COUNCILS, BOARDS AND COMMISSIONS

Councilmember Stokes complimented the Commission on the joint meeting with the Council. He said the Councilmembers are still talking about what they saw as a very positive experience. With regard to downtown livability, he said he looked forward to hearing what the public had to say about the proposed early wins. He also agreed with the public comments made relative to moving things forward in a timely manner. The work being done will have an impact on economic development, but it is all being done within the current boom portion of the economic cycle. Hopefully the work can be done and implemented during the current positive part of the cycle.

6. STAFF REPORTS

Senior Planner Mike Kattermann called attention to the award received by the city for the integrated outreach plan associated with the Comprehensive Plan update. He noted that the Council initiated the Comprehensive Plan update in October 2012 and adopted the update in August 2015. Public engagement was a cornerstone of the entire project; it included an emphasis on reaching diverse and underserved stakeholders. The overall outreach strategy was tailored to the different purposes and phases of the update to engage citizens in a wide range of environments. The outreach program included the online Bellevue's Best Ideas tool that generated 126 unique ideas and garnered over 1700 votes. The tool was promoted online through social media and by directly engaging the public at venues like Crossroads Mall, Downtown Park, the pedestrian corridor and the Microsoft store in Bellevue. The ideas were turned into draft policies, vetted, and further developed through six Bellevue boards and commissions.

The Planning Commission had the primary role of making sure all the pieces fit together. The draft plan with new and revised policies was presented to the community through in-person and online methods. In all, 75 community groups were contacted; a series of open houses and public forums were held; and there was wide distribution and solicitation of feedback through an online open house and social media. The online open house alone had over a thousand unique visitors, and the draft plan content received over 70,000 views.

The electronic outreach took the city to a new level of public engagement, and the Planning Commission was critical to the effort in its support and involvement in the outreach, and in ultimately putting together a plan that furthers the vision of the community.

On behalf of the Planning Association of Washington and the Washington Chapter of the American Planning Association, Mr. Steve Pilcher presented the award to Chair Hilhorst who accepted it on behalf of the city. He said the award program has been in place for 29 years and annually receives between 15 and 20 submittals in a number of categories. A jury of planning

professionals review the award submittals and determine who should receive the awards in various categories. He noted that with regard to Bellevue's submittal, the jury was very impressed with the level of participation and the different means utilized to involve the public.

In accepting the award, Chair Hilhorst stressed that it belongs to the members of the Planning Commission, the members of all the other boards and commission that were involved, and the staff who worked tirelessly in capturing thoughts and putting them into understandable line items for review. The award speaks volumes about the caliber of volunteers Bellevue has and the degree to which by working together everyone can have a say in the future of the city.

7. DRAFT MINUTES REVIEW

A. November 18, 2015

Chair Hilhorst called attention to the sixth paragraph on page 7 and noted that the reference to "Councilmember Wallace" should in fact read "Deputy Mayor Wallace."

A motion to approve the minutes as amended was made by Commissioner Laing. The motion was seconded by Commissioner Carlson and the motion carried without dissent; Commissioner Morisseau abstained from voting as she had not attended the meeting.

8. STUDY SESSION

A. Downtown Livability

Code Development Manager Patricia Byers called attention to page 18 of the packet and noted that a comment had been received about vocational schools being allowed in the O1 and O2 zones.

Ms. Byers reminded the Commissioners that the Council initiated the Downtown Livability Initiative and appointed a Citizen Advisory Committee to conduct public outreach and develop broad recommendations. The recommendations were forwarded to the Council which in turn directed them to the Commission to address. It was at the Commission level where the decision was made to focus on early wins first before moving on to the bulk of the recommendations. The Commission's recommendations regarding the early wins and the rest of the issues will ultimately be forwarded to the Council for deliberation and final action.

None of the selected early wins issues are intertwined with topics that will be addressed later. The early win topics were also deemed to be less complex, and they were consistent with direction from the CAC. The early wins are permitted uses, signage for public open spaces, mechanical equipment locations and screening, street trees and planter strips, the downtown boundary, weather protection, and extending the pedestrian corridor to 112th Avenue NE. The topics to be addressed in 2016 are height and form, the amenity incentive system, the design guidelines, public open space, and procedures.

A motion to open the public hearing was made by Commissioner Carlson. The motion was seconded by Commissioner deVadoss and the motion carried unanimously.

Mr. Patrick Bannon spoke representing the Bellevue Downtown Association, 400 108th Avenue NE, and noted that he had served as a member of the Downtown Livability Initiative CAC. He said BDA members have been engaged with the livability initiative at all levels since its inception and will follow through to the conclusion. One theme the BDA has been focused on

from the start is flexibility. The changes that are enacted should not preclude future viable and creative ideas. While it cannot be known in advance what some of those uses might be, there should be in place a review process to consider all opportunities. One example is manufacturing uses and the changes that are being considered. It makes good sense to eliminate some of the language regarding manufacturing uses that would be inappropriate for the downtown, but the language should be written so as not to preclude things like game development or technology applications. With regard to microbreweries, the definition should be drafted so as to not preclude micro-distilleries or wine making operations. The note tying microbreweries to eating and drinking establishments makes sense, but there should be some clarification as to whether that means eating “and” drinking establishments or if it could also mean eating “or” drinking establishments. With regard to drive-through uses in the downtown, there should be a clear rationale for limiting them in the future, but the code and the charts should not necessarily impair a creative application of a drive-through use as an accessory use outdoors.

Mr. Walter Scott with the Legacy Corporation, 400 112th Avenue NE, complimented the Commission for moving quickly ahead and for having some very good ideas. One example is the additional credits developers can gain for what amounts to a coolness factor relative to architecture that represents a well-placed element of trust in city staff. For the most part the area of the downtown that includes City Hall is a sterile environment, or put another way it is a clean sheet of paper. There has been a lot of attention focused on what should be built in the area. The city could benefit greatly by giving additional focus on what should not be allowed. Public safety is clearly important where plazas are concerned and the call for spaces to have 24-hour activation should be made very cautiously. Consideration should also be given to the benefit of public versus private plazas. With the latter there is a certain element of control, which is clearly desirable. Parking is a necessary evil, but it can benefit the downtown. It should be allowed in the perimeter districts and less so in the center of the city. Public amenities like transportation and meeting halls could benefit from having overflow parking provided by private companies, but exceptions should be made where there are problems with groundwater tables. Legacy Corporation focuses on retail and its closest property to the downtown is the Bellevue Design Market next to Whole Foods. The call for flexibility made by the BDA is laudable. A system that is more loosely regulated is preferred because it lets the market determine what is needed. Ultimately developers want a safe environment that will draw customers and families.

Mr. Carl Vander Hoek, 3421 2nd Avenue SE, called attention to the proposed land use chart changes relative to culture, entertainment and recreation, and said it appeared to him that the changes with regard to recreation activities such as miniature golf, tennis courts, community clubs, athletic fields, play fields and swimming pools for Old Bellevue and Office/Limited Business would be permitted outright rather than requiring administrative review. The attached note indicates the revision was requested by the parks department. It is not clear why the “A” was deleted and the “P” was inserted. He asked for clarification.

A motion to close the public hearing was made by Commissioner Laing. The motion was seconded by Commissioner Morisseau and the motion carried unanimously.

Commissioner Laing asked if there is something in the code language that allows the Director to determine if a proposed use not specifically spelled out in the code is similar to a use that is listed in the code. Land Use Director Carol Helland said there is and pointed him to section 20.10.014. Commissioner Laing suggested adding a reference to that section in 20.25A.015(A) Permitted Uses. Ms. Helland agreed to include it.

Commissioner Walter called attention to page 14 and the chart references to healthcare. She noted that there is a new model that is on the rise nationally and is being lobbied in Washington

state, namely freestanding emergency rooms. One such facility in Tampa Bay, Florida, accepts ambulances. She suggested either adding a separate category for freestanding emergency rooms, or have the use be the same disposition as a hospital. Allowing a use that could include ambulances rushing to them could be problematic for the notion of a pedestrian community in the downtown. The use certainly does not fit under the heading of other healthcare related services. If treated as a hospital, the use would be allowed only in a couple of specific areas. Community Development Manager Patti Wilma suggested allowing the use in Office/Limited Business might be reasonable given that district's proximity to the freeway.

Commissioner Walter clarified for Commissioner Carlson that she wanted a freestanding emergency room use classified as a hospital rather than a drop-in medical clinic.

Addressing the comments made by Mr. Vander Hoek, Chair Hilhorst asked if in fact the downtown could be expanded to allow more recreational uses, such as tennis courts and lighted facilities. Ms. Byers said the previous footnote indicated administrative use approval was required in the DNTN-R district where lighting or amplified sound was involved. A non-recreational use put into a park required conditional use permit approval. The move toward a more permissive code, and the desire to encourage more open space, is the reason for the proposed approach. Everything that occurs in the downtown is subject to design review, which addresses traffic and parking, but not lighting and amplified sound. By including a requirement for administrative conditional use, uses that could affect the residential districts can be given more scrutiny.

Ms. Wilma added that events that occur in Downtown Park and Old Bellevue are generally required to obtain special events permits, which is a separate process and analysis that involves consideration by all city departments. Under the proposed approach, a lighted tennis court or playfield would require administrative conditional use. Ms. Byers said the process would not preclude lighting or amplified sound, but it could result in certain restrictions, such as limiting the hours during which the lighting and sound is allowed. Things like special events generally require a special events permit, but something like a ballfield would not, and the issues of lighting and sound would not be addressed unless subject to administrative conditional use.

Commissioner Laing called attention to Footnote 2 on page 9 and asked if the intent is to omit the size limitation or to limit to a maximum of 2000 square feet per establishment. He also said it was unclear what is meant by "establishment." Ms. Wilma said Footnote 2 applies on Page 8. Ms. Byers said the 2000-square-foot limit was originally proposed, and the requested revision was to delete that.

Commissioner deVadoss referenced Page 14 and reference number 639 relating to rental and leasing services and noted that new virtual uses are emerging that do not necessarily have an associated physical infrastructure. Accordingly, the use should be clarified in the chart. Ms. Wilma said virtual rental and leasing services would fall under the administrative office category, and it would be up to the Director to make the determination what the actual business is.

Chair Hilhorst observed that on the transportation and utilities chart, on-sight hazardous waste treatment and storage facilities are currently permitted with an administrative conditional use permit. Ms. Byers said the category is entirely related to hazardous waste and the proposal is to eliminate the use. Storage facilities is a different use.

Commissioner Walter pointed out that some medical facilities use radioactive pharmaceuticals and the like and she asked if they are considered hazardous materials. Ms. Byers said hazardous

waste has a specific definition in the RCW. Waste generated by medical facilities are a subordinate use and would still be allowed.

Commissioner Carlson referred to allowing microbreweries so long as they are combined with an eating and drinking establishment. He asked about eating establishments connected with an on-site distillery or winery. Ms. Byers said that would fall under the category of unclassified uses. A coffee roaster use wanting to include a café would be the same thing. The Director may treat an unclassified use as a similar classified use. So, in this instance an eating and drinking establishment with a micro-distillery would be treated the same as an eating and drinking establishment with a microbrewery. As drafted, there is flexibility to locate a distillery or winery in the downtown.

With regard to drive-through banking facilities, Commissioner Walter said she contacted a bank manager friend to ask about the logistics and safety of locating a drive-through in a parking garage. The response was that with banking moving more to electronic banking, the logistics of building a drive-through in a parking facility would be highly unlikely. Additionally, there can be a lot of congestion within a parking garage with cars trying to get in and out, particularly toward the end of a business day. She suggested the drive-through banking issue should be reconsidered in light of all the complications. Ms. Wilma said her research of drive-through banking facilities turned up a number of articles focused on the fact that the use is dying out. Some 70 percent of banking is now done online, and many financial institutions are choosing to close their drive-through windows. Requiring them to be below ground will hasten their demise, but the benefit of achieving more open space, which will make the downtown more livable, has the greater weight. Commissioner Walter proposed continuing to allow drive-throughs as they are currently on the assumption that the use will die out over time of its own accord. There could be a new type of business in the future for which drive-throughs would make a great deal of sense, but as proposed they could not even be considered.

Commissioner Laing commented that where there is a valid police power reason to regulate drive-throughs, it does not matter what type of business is involved, be it a bank, a pharmacy or some future unknown use. It would be arbitrary to single out bank drive-throughs while allowing pharmacy drive-throughs. If drive-throughs are to be allowed in a parking structure, any use should be allowed to have them. The easiest way to address them would be to simply say drive-throughs for any use are permitted in structured parking in specific zones. Additionally, if a drive-through is to be located outside of structured parking, there should be screening and other requirements imposed.

Commissioner Carlson noted that some types of drive-throughs have not been allowed in the downtown for years. Ms. Wilma said food drive-throughs are prohibited.

Commissioner Laing said just because something has been done in a certain way does not mean that is the way it should continue to be done. There is simply no good reason for drawing a distinction between drive-throughs based on what they dispense. Ms. Wilma said the studies done concerning obesity and public health have driven reductions in the number of food drive-throughs across the country. Discouraging congestion in downtown areas is another big factor.

Chair Hilhorst said she was not willing to legislate how people live their lives. However, there is logic in keeping cars and people separate from each other. The code should include flexibility for unforeseen future businesses. For many, drive-throughs represent a convenience and in fact a need. Ms. Wilma reiterated that as drafted the proposal allows drive-throughs.

Ms. Helland said drive-throughs associated with food have not been allowed in the downtown

since the code was written. Congestion is one of the evils the downtown faces; if it continues to worsen, the pedestrian environment of the downtown will suffer. The proposed approach does not seek to eliminate cars, but it does seek to eliminate things that create undo congestion at the expense of the pedestrian environment. To allow drive-throughs associated with food would be putting something back that would then create a new challenge through the permitting process. When Chick Fil A opened in Wilburton, there were long lines of cars backing up through the intersection that required a great deal of recalibrating. Once a use is allowed, it is much more difficult to remove. Congestion also increases emissions from cars, which is yet another health concern. The proposal allows for drive-through facilities if they are located away from the pedestrian environment. The approach has met with success in other jurisdictions. She agreed that no distinction should be made with regard to what is dispensed from drive-through windows provided they are located in structured parking. The nonconforming use provisions do not let existing drive-throughs swap out to a new use, say from a bank to a pharmacy. The section could be drafted, however, to allow drive-through uses to continue provided they meet certain characteristics.

Commissioner Carlson asked if drive-through coffee businesses are allowed. Ms. Byers said they are considered eating and drinking establishments and as such drive-throughs associated with them are not permitted. The code could be revised to allow them in structured parking.

There was agreement drive-throughs associated with cannabis businesses should not be allowed.

Ms. Helland commented that over time the Commission has grown very fond of the comment boxes usually included as part of the code review process. Creating them, however, increases the code drafting time significantly. The new approach seeks to shorten that time, in part because of the Commission's stated desire to finish the review by the end of the year. It would be helpful if the Commissioners were to submit their comments for inclusion in the next packet, with comments regarding content and grammatical revisions noted separately.

Commissioner Barksdale asked if there is a way by which the comments offered by the Commissioners can be shared publicly, with the public allowed to comment as well. Ms. Helland said the technique works well as part of outreach efforts, but in order to stay in line with the requirements for public meetings, the Commissioners could not engage in what amounts to a serial public meeting.

The Commissioners worked through the document page by page. With regard to page 4 there was agreement to include a cross reference to 20.10.014 as previously discussed.

With regard to Footnote 2 on page 8, Ms. Helland noted the Bellevue Downtown Association's request to remove the previously existing 2000-square-foot limitation. There was consensus to accept the suggestion of the BDA.

Commissioner Laing called attention to page 11 and observed that under congregate care senior housing there is a reference to Footnote 1 which requires the recording of an agreement restricting senior dwellings or congregate senior housing to remain in perpetuity. However, in *City of Olympia vs Paulser* makes it clear zoning authority cannot be used to impose perpetual use restrictions on property. He suggested the footnote should be redrafted to comply with the court's finding. The idea in the Paulser case is that a future council's legislative authority cannot be restricted with regard to changing the use of a property. Ms. Helland said that was a good catch and noted that the change had been made elsewhere in the code to refer to the life of a project rather than in perpetuity. She said she would make the change on page 11 as well.

Commissioner Laing noted that the comments made by the BDA relative to drive-throughs refers to page 13 and the need to delete Footnote 11 and add Footnote 10 allowing drive-throughs in structured parking as an accessory use. He said he agreed with the comment and recommended making the change. Ms. Helland agreed to put Footnote 10 in all places Footnote 11 appears in the draft.

With regard to page 14, Ms. Helland addressed the issue of freestanding emergency rooms by noting that the healthcare industry is very specific in the state of Washington and there is no such thing as standalone emergency rooms, only urgent care facilities. She proposed stating that emergency rooms will only be permitted with hospitals. Commissioner Walter said healthcare nationwide is changing rapidly in ways that cannot be imagined. That is why something should be included prohibiting freestanding emergency rooms independent of hospitals. Ms. Helland reiterated her call to allow emergency rooms only in conjunction with hospitals in hospital district and to address any future changes as they arise. There was consensus to take that approach.

Also on page 14, Commissioner Laing agreed with the staff recommendation regarding pet grooming and pet daycare being permitted outright where less than 2000 square feet, and by administrative conditional use where the use exceeds 2000 square feet.

Ms. Byers called attention to the special schools use on page 15 and noted a request that they be permitted outright in the O1 and O2 zones. The Commissioners agreed to make that change to the chart.

Commissioner Laing called attention to the new Footnote 3 on page 16 and pointed out the need to insert the word “and” following the code reference and “only if located.”

Commissioner Laing referenced the gasoline service station use on page 24 and asked if there is an existing station in the Old Bellevue district. Ms. Wilma allowed that there is and noted that it is a legal nonconforming use.

With regard to the veterinary clinic and hospital use listed on page 29 and the associated Footnote 2 pertaining to Old Bellevue, Commissioner Laing said his preference would be to permit the use outright up to 2000 square feet and through an administrative conditional use permit above 2000 square feet. He proposed using the same language as the comment on page 14.

Commissioner Walter called attention to item 3.c on page 35 and suggested that “designed to form logical routes” could be interpreted to mean different things to different people. She also suggested the word “diversity” as used in item 3.d is vague. Ms. Byers pointed out that the only change from the existing code is use of the term “through-block pedestrian connections” rather than “pedestrian connections.” Commissioner Walter proposed using “variety” or something broader in 3.d. The Commissioners proposed “direct,” “clear” and “shortcut” for 3.c.

Commissioner Laing observed that the footnote referred to item 3.h on page 35 calls for striking the highlighted sentence, but it is unclear exactly what is being referred to. He proposed replacing the sentence that begins with “the Director shall specify” with the contents of the comment box, and leaving the last sentence as it is. Ms. Byers said that was the intention.

A motion to extend the meeting to 9:15 p.m. was made by Commissioner Laing. The motion was seconded by Commissioner deVadoss and the motion carried unanimously.

Commissioner Laing drew attention to page 36 and item c.ii, hours of operation of minor publicly accessible spaces. He recommended replacing “greater” with “lesser” to always have eyes on the site. The comment is in regard to pedestrian bridges but there is an important difference in that they exist over city rights-of-way and are allowed in the public space subject to certain conditions. McCormick Park and Ashwood Park in the downtown have hours of operation of dawn to dusk, which is reasonable. Downtown Park has dawn to 11:00 p.m. as its hours of operation. As drafted, c.ii would require a private property owner to keep its property open to the public for more hours than the city keeps its own parks open to the public. Using “lesser” will present a much fairer approach. Ms. Byers said staff looked at the minor publicly accessible spaces that are in existence or close to being in existence and found that in almost every case they are on or close to corners very close to public rights-of-way. People will naturally assume that a publicly accessible space located adjacent to a corner and having a bench on it is in fact public property. Commissioner Laing pointed out that the proposed code change will not be applied retroactively, so for those existing spaces the code will require them to be open at least during normal business hours. The private property owner is ultimately responsible for what occurs on publicly accessible spaces, and they should have the ability to ask folks to vacate the space outside of normal business hours.

Commissioner Carlson said he could support establishing the hours as dawn to 10:00 p.m. Other cities have adopted a shoulder shrugging attitude that has led to the problems they currently face. Bellevue should be proactive in making sure property owners can shoo people out after hours as a way to prevent loitering, vandalism and criminal activities.

Ms. Helland said her preference would be to indicate specific times, such as the greater of normal business hours or 8:00 a.m. to 10:00 p.m. There was agreement to make that change.

Commissioner Laing reiterated his comment about signage relative to item iv on page 39.

There was agreement to have staff draft and circulate to the chair and vice-chair the transmittal memo for review and agreement, allowing the packet to be forwarded to the Council without having to take time at the Commission’s next meeting.

B. Eastgate/I-90 Corridor Implementing Regulations

Senior Planner Erika Rhett explained that development agreements were authorized by the state legislature in 1995. Concomitant agreements were the construct used before there were development agreements. In short, they involve regulations that are specific to a particular site. They are applied at the time of zoning or rezoning and they remain with the site until they are amended or repealed. Concomitant agreements sit at the level of regulations and supersede all regulations in cases of inconsistency. If a concomitant agreement says something cannot be done that is otherwise allowed by the underlying zoning, the thing cannot be done.

Much of the Eastgate area is covered by concomitant agreements. The Eastgate/I-90 CAC recommended repealing them all as they are an added layer of regulation that adds complexity for development that is usually unnecessary. Many of the concomitant agreements were put in place before the types of rules and regulations now in place even existed. As they stand, they could prevent a full implementation of the corridor as envisioned by the CAC.

In some instances, the concomitant provisions are redundant to citywide regulations that were subsequently enacted. A typical example is in transition areas where concomitant agreements spell out exactly what the Transition Area Design District or Critical Areas regulations now address. There are also provisions that talk about compliance with state and federal requirements,

something that is required regardless of any concomitant agreement.

There are also some one-time requirements spelled out in concomitant agreements. A good example is the I-90 Office Park which requires the construction of several public roads in order to implement the development. The roads have all been completed and should anyone seek to redevelop the site an extensive public process would be required to get rid of the public right-of-way. The one-time improvements agreements are no longer needed.

There are also provisions that are inconsistent with the Eastgate vision. Eastgate Plaza is a good example in that only a handful of uses is allowed there under the concomitant agreement that is in place. The wide range of uses talked about and recommended by the CAC relative to the Neighborhood Mixed Use zoning would not be allowed unless the concomitant agreement is repealed, including residential development.

Ms. Rhett noted that there are some provisions that should be retained. One way to accomplish that would be to add them to the code. Examples include provisions that talk about things like design review or master plan developments.

A few things will need a more careful review, and in approaching the code and talking about the specifics, they will be pulled out for more detailed discussion. Such provisions are typically specific to individual sites and include affordable housing requirements on the Sunset Corporate Campus, and specific environmental and traffic monitoring requirements in the I-90 Office Park. Staff are working with the city attorney's office on a strategy that will allow the provisions to be retained once the concomitant agreements are eliminated.

Ms. Rhett said staff are also working on an outreach strategy that will involve a stakeholder analysis. Each one of the concomitant agreements in place have a specific group of stakeholders and a targeted outreach is planned to make sure everyone is talked with about the agreements. The code language will be brought forward in 2016 along with specific provisions to incorporate into the Land Use Code, and as many of the concomitant agreements as possible will be addressed.

Commissioner deVadoss asked how many concomitant agreements are entered into in a typical year. Ms. Rhett said the city no longer enters into concomitant agreements, though it does on occasion enter into development agreements. The full breadth of regulations now in place that address design, critical areas, transition areas and the like, there is far less need for property specific agreements.

Ms. Rhett said the preliminary information from the city attorney is that if there is going to be a legislative rezone for the Eastgate/I-90 corridor that will involve repealing the concomitant agreements, all of the agreements subject to the rezone will need to be repealed; it will not be possible to pick and choose. If there are provisions housed in certain existing concomitant agreements, it will be necessary to either take a different legal course or not repeal the agreements.

Chair Hilhorst asked if the Eastgate area has more concomitant agreements than other areas of the city. Mr. Rhett allowed that it does and likely has to do with the vintage of the first wave of development there.

Commissioner Morisseau commented that some of the issues addressed by the concomitant agreements, such as affordable housing, overlap with items the Commission will be working on in the future. She asked what steps will be taken to ensure that decisions made relative to

Eastgate/I-90 will mesh with the decisions to be made in later studies. Mr. Rhett said as issues such as affordable housing are addressed relative to the Eastgate/I-90 code, the Commission will be asked to carefully consider the broader implications.

Commissioner Laing said one approach would be to leave the concomitant agreements in place and move ahead with rezone actions that say once redevelopment occurs the concomitant agreements are voided. That would keep in place the mitigations and protections for the specific projects to which the agreements apply. Redevelopment under the new zoning would eliminate the concomitant agreement and the new code provisions would apply. Mr. Rhett said that would be a viable strategy in the event the concomitant agreements are not simply eliminated.

C. Eastside Rail Corridor

Senior Planner Camron Parker with the Department of Parks and Community Services shared with the Commissioners a map showing the extent of the Eastside Rail Corridor. The section through Bellevue stretches some seven miles and runs north and south and intersects and connects with several non-motorized transportation facilities, including the SR-520 trail and the Mountains to Sound Greenway. The opportunity exists to connect a future trail along the rail corridor to the city's parks and trail system. Many of the major parks in Bellevue, such as Coal Creek, Mercer Slough, Wilburton Hill and the Bellevue Botanical Garden, are aligned such that providing connections between them and the corridor could be accomplished relatively easily. Additionally, the grand connection concept has the rail corridor as its eastern terminus; the upcoming Wilburton land use and transportation project will involve the corridor; and the light rail station to be constructed in Wilburton all point to the need to get a step up in planning for connections with the corridor. To that end the Council has put a \$1 million placeholder in the CIP to further collaborative efforts.

The Eastside Rail Corridor has its northern terminus in the city of Snohomish and its southern terminus in Renton. For the most part, King County owns the portion of the corridor located in King County, though Sound Transit owns two miles of the segment running through Bellevue. The city of Bellevue does not own any of the corridor. Along the corridor there are a series of easements that exist. There is an easement along the corridor for a trail; Puget Sound Energy has a utility easement; and Sound Transit has a high-capacity transit easement. Additionally, the ability to take the corridor and keep it intact is provided through the federal rail banking program, which means should a viable freight rail use return, it would have some rights to use of the corridor.

Mr. Parker said there are in fact more easements in the corridor than there is land to put them, which is a complicated tangle that will need to be sorted out. To make sense of it all, a regional advisory council for the Eastside Rail Corridor has been formed. The members include the owners, namely King County, Sound Transit, the cities of Redmond and Kirkland, and Puget Sound Energy. The advisory council has been in place for a few years but now that discussions are moving into the future of the corridor, King County is looking to restructure the council to permit additional stakeholders, including the city of Bellevue, to play a greater role.

Commissioner Morisseau asked why the city does not own any of the corridor. Mr. Parker explained that Burlington Northern/Santa Fe sold the entire corridor to the Port of Seattle, which subsequently sold chunks of it to different entities. King County owns the bulk of it and made the purchase based on its vision of having regional trails throughout the county.

Answering a question asked by Commissioner deVadoss about the utility easement in the corridor, Mr. Parker said the fact that the corridor stretches north and south in a single piece over

a long distance makes it very attractive to regional utility providers, not necessarily for planned projects but rather for future flexibility.

Mr. Parker said the key element of the various potential uses along the corridor that is getting the most focus currently is the development of a trail. King County is in the midst of producing a trail master plan. A draft plan is expected in 2016 along with an associated environmental impact statement. In general, King County is looking at a trail alignment that uses the bed of the existing rail, and one that would be off of the existing rail bed. A preferred alternative will be identified that involves one or the other or a combination of both.

Senior Transportation Planner Mike Ingram said in conjunction with the trail master planning process, Bellevue staff have been in discussion with the owners of the rail corridor in Bellevue, namely King County and Sound Transit, regarding opportunities to implement interim trail improvements. Kirkland has done similar things by removing the rails and ties, smoothing out the ballast rock, and applying a course of crushed gravel, the result of which has been a satisfactory trail surface. The projects and conditions are very promising in the northern part of Bellevue relative to the segment from the boundary with Kirkland to SR-520, and the segment from SR-520 to NE 8th Street. The King County Council recently approved funding to remove the rails through Bellevue and indeed all the way south to Renton, and funding for interim trail improvements between 108th Avenue NE and SR-520. The work will include a connection to Northup Way, which will serve as the primary east-west bicycle route through the northern part of the city.

Commissioner Barksdale asked how removing the rails keeps the corridor available for use as a future freight rail operation. Mr. Ingram explained that the current rails are not usable for freight or rail transit.

The expectation is that King County will remove the existing rails and implement the interim trail by the end of 2017. With regard to the segment between SR-520 and NE 8th Street, Sound Transit will be developing interim trail improvements in conjunction with the Memorandum of Agreement signed with Bellevue.

A motion to extend the meeting to 9:25 p.m. was made by Commissioner deVadoss. The motion was seconded by Commissioner Morisseau and the motion carried unanimously.

Mr. Ingram noted that certain locations present challenges. The NE 8th Street crossing is one and King County staff are currently analyzing what it would take to create an overpass for the trail while still meeting all functional access needs. The city recognizes the prominence of the location and the opportunity to implement a connection that will have an aesthetic quality and reinforce the identity of the Wilburton area and the rail corridor itself. In the coming year the city will collaborate with King County and Sound Transit. The hope is that the crossing will be operational by the time East Link is operational in 2023.

Another challenge is the gap over I-405 southbound. The expansion of I-405 in 2008 triggered the loss of the rail corridor bridge over the freeway. How to reconnect the missing section will be a key consideration. WSDOT is committed to restoring the connection, but what it will look like is yet to be determined. The hope is the project can be tied to and completed in conjunction with the Bellevue-to-Renton I-405 expansion project which is funded and slated for construction beginning in 2019.

Mr. Ingram reminded the Commissioners about the upcoming summit event on January 9 that is aimed at bringing in the broader community and building excitement around the trail and

opportunities along the corridor. Key speakers will be brought in to talk about examples from around the nation of how investment from a number of parties have helped bring projects online. As the planning work moves forward, it will be meshed with the grand connection concept and Wilburton land use update work.

Commissioner Morisseau asked how the city intends to mitigate any potential issues that might be tied to having a trail adjacent to a reactivated rail line. Mr. Parker said should a rail use come forward, the trail in most instances would be replaced by the rail line. Most experts hold the view that the section through Bellevue will never see a freight rail use again in the future, but the potential for it will need to be considered, thus steps will be taken to leave an envelope for it. Mr. Ingram added that the county's master planning process will be looking at how to fit everything into the corridor to the degree possible, including potential freight rail.

9. PUBLIC COMMENT – None

10. ADJOURN

A motion to adjourn was made by Commissioner Laing. The motion was seconded by Commissioner Morisseau and the motion carried unanimously.

Chair Hilhorst adjourned the meeting at 9:24 p.m.